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YEAR	PROJECT NO.	SHEET NO.
2025	PROT-29(131)	ROADWAY-SIGN
<p align="center">STATE OF TENNESSEE DEPARTMENT OF TRANSPORTATION</p>		
<p align="center">SIGNATURE SHEET</p>		

Index Of Sheets
SEE SHEET NO. 1A

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF ENGINEERING

SCOTT COUNTY

SR-29 (US-27, ALBERTA ST): M20SAHQ R1SR SR029 18.75 LM
SLOPE CORRECTION

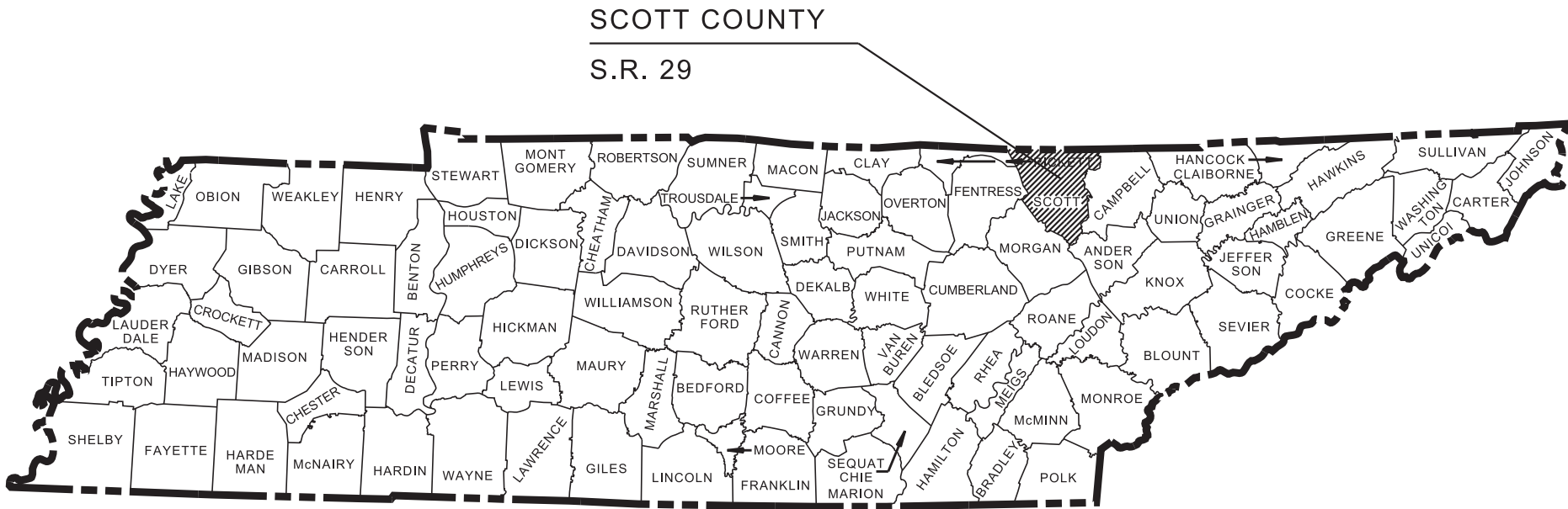
PS&E

GRADE, DRAIN, WIDENING, GUARDRAIL, RESURFACING, AND SLOPE REPAIR

STATE HIGHWAY NO. SR-29 F.A.H.S. NO. US-27

DOES THIS PROJECT QUALIFY FOR UTILITY CHAPTER 86	YES	NO X
WORK ZONE SIGNIFICANCE DETERMINATION		
SIGNIFICANT	YES	NO X

TENN.	YEAR	SHEET NO.
	2025	1
FED. AID PROJ. NO.	PROT-29(131)	
STATE PROJ. NO.	76001-3289-14	



76001-3289-14
END PROJECT NO. PROT-29(131)
STA. 115+00.00 CONSTRUCTION
N 794190.8443 E 2405250.9983

END PROJECT NO. 76S029-M2-003
STA. 114+44.02 R.O.W.
N 794138.4851 E 2405231.1923

BEGIN PROJECT NO. 76S029-M2-003
STA. 111+85.00 R.O.W.
N 793896.2188 E 2405139.5495

76001-3289-14
END PROJECT NO. PROT-29(131)
STA. 106+50.00 CONSTRUCTION
N 793391.0813 E 2404964.8719

SPECIAL NOTES

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES
CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW
THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF
THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED JANUARY 1, 2021 AND
ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS
AND IN THE PROPOSAL CONTRACT.

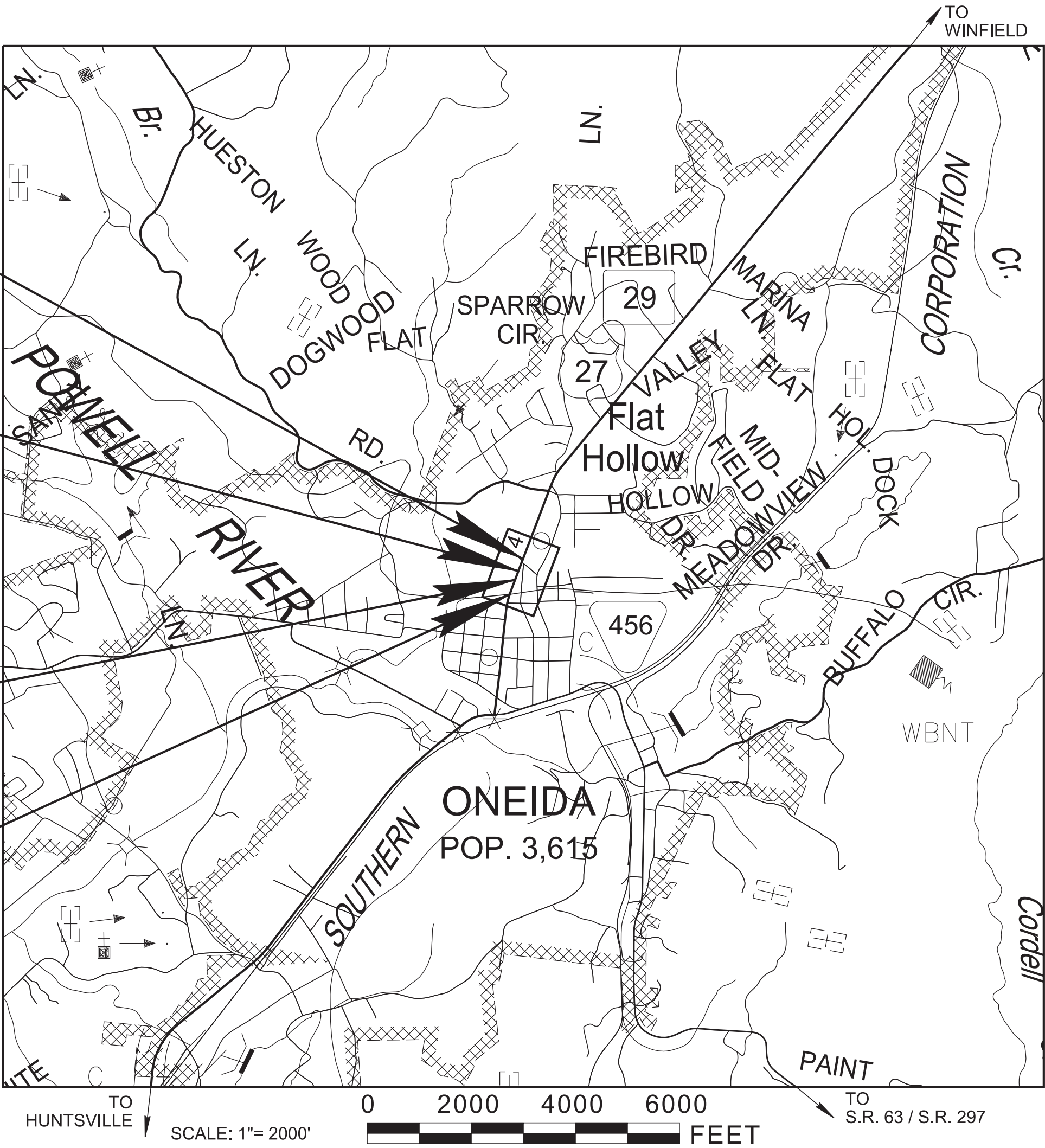
TDOT MANAGER : MR. RANDY BUSLER, P.E.

DESIGNER : ERIC KIM, P.E.

CHECKED BY : MR. STACY WEAVER, P.E.

P.E. NO. 76001-4288-04 (DESIGN)

PIN NO. 130146.00

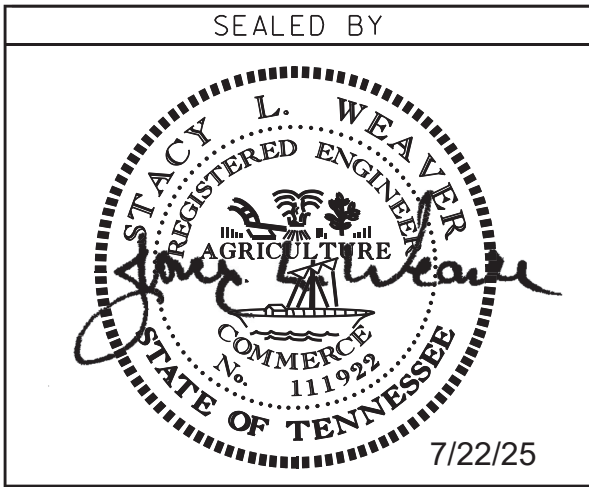


R.O.W. LENGTH	0.049 MILES
ROADWAY LENGTH	0.160 MILES
BRIDGE LENGTH	0.000 MILES
BOX BRIDGE LENGTH	0.000 MILES
BOX BRIDGE LENGTH	0.000 MILES ▲
PROJECT LENGTH	0.160 MILES

▲ Not included in the project length (Non Riding Surface).

NO EXCLUSIONS

NORTH CROSS STREET TO
BE CLOSED DURING
CONSTRUCTION. SEE SHT. T3.



APPROVED:
WILL REID, CHIEF ENGINEER

DATE:

APPROVED:
WILL REID, COMMISSIONER

S.R.29 8-8-2024

SURVEY 11-09-20	TRAFFIC DATA
	ADT (2025) 16980
	ADT (2045) 18670
	DHV (2045) 1867
	D 65 - 35
	T (ADT) 12 %
	T (DHV) 8 %
	V 35 MPH

COORDINATES ARE NAD 83(2011), ARE DATUM ADJUSTED BY THE
FACTOR OF 1.00005 AND TIED TO THE TGRN. ALL ELEVATIONS
ARE REFERENCED TO THE NAVD 1988 WITH GEOID 18.

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ROADWAY INDEX

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GEOTECHNICAL PLANS	G-1
UTILITY PLANS.....	U1-1
NO PROJECT COMMITMENTS SHEET INCLUDED IN THIS SET OF PLANS.	
NOTE: THE ALPHABETICAL LETTERS “I”, “O” & “Q” ARE NOT USED IN THE NUMBERING OF SHEETS.	

STANDARD ROADWAY DRAWINGS

DWG.	REV.	DESCRIPTION
STANDARD ROADWAY TITLE SHEET, ABBREVIATIONS, AND LEGENDS		
RD-A-1	02-20-20	STANDARD ABBREVIATIONS A THROUGH L
RD-A-2		STANDARD ABBREVIATIONS M THROUGH Z
RD-L-1	02-20-20	STANDARD LEGEND
RD-L-1A		STANDARD LEGEND
RD-L-2	02-20-20	STANDARD LEGEND FOR UTILITY INSTALLATIONS
RD-L-5	07-30-24	STANDARD LEGEND FOR EROSION PREVENTION AND SEDIMENT CONTROL
RD-L-6	02-20-20	STANDARD LEGEND FOR EROSION PREVENTION AND SEDIMENT CONTROL
RD-L-7	02-20-20	STANDARD LEGEND FOR EROSION PREVENTION AND SEDIMENT CONTROL
ROADWAY DESIGN STANDARDS		
RD11-SE-1		TRANSITION AND CROSS SLOPE DETAILS
RD11-SE-2		SUPERELEVATION TRANSITION DETAILS FOR UNDIVIDED ROADWAYS
RD11-SE-2A		SUPERELEVATION TRANSITION SECTIONS FOR UNDIVIDED ROADWAYS
RD11-TS-3		DESIGN STANDARD FOR ARTERIAL HIGHWAYS (2-LANE)
RD11-TS-7B	07-17-20	DESIGN STANDARDS 2-LANE HIGHWAYS WITH CONTINUOUS 2-WAY LEFT-TURN LANE
RD11-LR-1		MINIMUM RUNOFF LENGTHS (LR) FOR URBAN HIGHWAYS
RD11-S-11		DESIGN AND CONSTRUCTION DETAILS FOR ROADSIDE SLOPE DEVELOPMENT
RD11-S-11A		ROADSIDE DITCH DETAILS FOR DESIGN AND CONSTRUCTION
RD11-SD-1		INTERSECTION SIGHT DISTANCE DESIGN AND GENERAL NOTES
RD11-SD-2		INTERSECTION SIGHT DISTANCE LANDSCAPE AND OBSTRUCTION
RD11-SD-3		INTERSECTION SIGHT DISTANCE 2-LANE ROADWAYS
PIPE CULVERTS AND ENDWALLS		
D-PB-1	03-01-23	STANDARD DETAILS FOR CONCRETE PIPE INSTALLATION
D-PE-4	06-28-19	STRAIGHT CONCRETE ENDWALLS (PIPE SIZES 18" TO 30")

DWG.	REV.	DESCRIPTION
SAFETY DESIGN AND GUARDRAILS		
S-CZ-1	06-28-19	CLEAR ZONE CRITERIA
S-PL-1	03-01-23	SAFETY PLAN FOR BARRIER LENGTH OF NEED
S-PL-6	07-30-24	SAFETY PLAN SAFETY HARDWARE PLACEMENT ON OUTSIDE EDGE
S-PL-7		SAFETY PLAN HARDWARE PLACEMENT AT INTERSECTIONS
S-GR31-1	03-13-25	GUARDRAIL DETAILS
S-GR31-1A	06-28-19	GUARDRAIL AND BLOCK-OUT DETAILS
S-GR31-1B		GUARDRAIL FASTENING HARDWARE
S-GR31-1C	07-07-23	GUARDRAIL GENERAL NOTES AND POST DETAILS
S-GRS-7	01-09-24	SHORT – RADIUS GUARDRAIL SYSTEM (SRGS)
S-GRS-7A	07-30-24	SHORT – RADIUS GUARDRAIL SYSTEM (SRGS) DETAILS
S-GRS-7B		SHORT RADIUS-GR SYSTEM (SRGS) EYE-BOLT SPACING DETAILS
S-GRS-8		SAFETY PLAN AT SIDEROADS OR DRIVEWAYS FOR LOW SPEED ROADWAYS
S-GRT-2	06-28-19	TYPE 38 GUARDRAIL END TERMINAL
S-GRT-2P	10-16-20	EARTH PAD FOR TYPE 38 AND TYPE 21 TERMINAL
S-GRT-3	06-28-19	TYPE 21 GUARDRAIL END TERMINAL
EROSION PREVENTION AND SEDIMENT CONTROL		
EC-STR-8	06-10-14	FILTER SOCK
EC-STR-19	04-01-08	CATCH BASIN PROTECTION
EC-STR-25	08-01-12	TEMPORARY CULVERT CROSSING, CONSTRUCTION EXIT, CONSTRUCTION FORD
EC-STR-34	05-04-22	EROSION CONTROL BLANKET FOR SLOPE INSTALLATION
EC-STR-37	06-10-14	SEDIMENT TUBE
MANHOLES		
D-MH-2	02-20-20	STANDARD PRECAST NO. 3 MANHOLE
D-MH-3	02-20-20	TYPICAL DESIGN OF LIDS FOR NO. 3 MANHOLE
D-MH-4	02-20-20	STANDARD NO. 3 MANHOLE CASTING AND STEPS
D-RMH-1		PRECAST MANHOLE STRUCTURES (48" THRU 120")

TYPE	YEAR	PROJECT NO.	SHEET NO.
PS&E	2025	PROT-29(131)	1A

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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ROADWAY INDEX
AND
STANDARD
ROADWAY
DRAWINGS

TYPE	YEAR	PROJECT NO.	SHEET NO.
PS&E	2025	PROT-29(131)	1A3


STANDARD TRAFFIC DESIGN DRAWINGS

DWG. NO. REV. DESCRIPTION

DESIGN - TRAFFIC CONTROL

T-M-1	01-24-25	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS AND MARKING ABBREVIATIONS
T-M-2	01-24-25	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS
T-WZ-10	03-26-25	ADVANCE ROAD WORK SIGNING ON HIGHWAYS AND FREEWAYS
T-WZ-11	03-26-25	ONE LANE CLOSURE DETAIL ON DIVIDED HIGHWAYS
T-WZ-16	03-26-25	LANE SHIFT FOR DIVIDED HIGHWAYS AND FREEWAYS
T-WZ-18	03-26-25	SHOULDER CLOSURE DETAIL FOR FREEWAYS AND DIVIDED HIGHWAYS
T-WZ-21	03-26-25	LANE CLOSURE WITH LEFT HAND MERGE AND LANE SHIFT
T-WZ-FAB1	03-26-25	FLASHING YELLOW ARROW BOARD
T-WZ-PBR1	03-26-25	INTERCONNECTED PORTABLE BARRIER RAIL
T-WZ-PBR2	03-26-25	DETAILS FOR WORK ZONE CHANNELIZATION DEVICES
T-WZ-PCB3	03-26-25	PORTABLE CONCRETE BARRIER RAIL DETAILS

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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

STANDARD
TRAFFIC
DESIGN
DRAWINGS

ESTIMATED ROADWAY QUANTITIES			
ITEM NO.	DESCRIPTION	UNIT	QUANTITY 76001-3289-14
105-01	CONSTRUCTION STAKES, LINES AND GRADES	LS	1
201-01	CLEARING AND GRUBBING	LS	1
202-02.01	REMOVAL OF PIPE (30" CMP, STA.113+62.80)	L.F.	8
202-02.02	REMOVAL OF PIPE (18" CMP, STA.113+62.80)	L.F.	6
202-01.56	REMOVAL OF STRUCTURES & OBSTRUCTIONS (1000 SF CONCRETE PAD)	LS	1
202-01.57	REMOVAL OF STRUCTURES & OBSTRUCTIONS (360 SF WOOD PORCH)	LS	1
202-01.58	REMOVAL OF STRUCTURES & OBSTRUCTIONS (79 LF OF CHAIN LINK FENCE)	LS	1
202-06.01	REMOVAL OF BUILDINGS (6574 SF, 2 STORY CONCRETE BLOCK TRACT NO. 3)	LS	1
(1)(11) 203-01	ROAD & DRAINAGE EXCAVATION (UNCLASSIFIED)	C.Y.	223
203-03	BORROW EXCAVATION (UNCLASSIFIED)	C.Y.	2186
203-06	WATER	M.G.	6
203-07	FURNISHING & SPREADING TOPSOIL	C.Y.	144
208-01.05	BROOMING & DEGRASSING SHOULDERS	L.M.	0.2
(2)(3) 209-03.22	FILTER SOCK (18 INCH)	L.F.	320
(2)(3) 209-05	SEDIMENT REMOVAL	C.Y.	6
(2)(3) 209-40.33	CATCH BASIN PROTECTION (TYPE D)	EACH	4
303-01	MINERAL AGGREGATE, TYPE A BASE, GRADING D	TON	260
402-01	BITUMINOUS MATERIAL FOR PRIME COAT (PC)	TON	4
(2)(3) 402-02	AGGREGATE FOR COVER MATERIAL (PC)	TON	4
403-01	BITUMINOUS MATERIAL FOR TACK COAT (TC)	TON	3
411-01.10	ACS MIX(PG64-22) GRADING D	TON	290
415-01.01	COLD PLANING BITUMINOUS PAVEMENT	TON	287
(15) 607-03.02	18" CONCRETE PIPE CULVERT (CLASS III)	L.F.	25
611-01.02	MANHOLES, > 4' - 8' DEPTH	EACH	1
(5) 611-07.01	CLASS A CONCRETE (PIPE ENDWALLS)	C.Y.	1
(5) 611-07.02	STEEL BAR REINFORCEMENT (PIPE ENDWALLS)	LB.	45
(12) 621-05.01	TEMPORARY SHORING	S.F.	3450
705-06.01	W BEAM GR (TYPE 2) MASH TL3	L.F.	325
705-06.14	SHORT- RADIUS GUARDRAIL SYSTEM (8FT RADIUS) MASH TL-3	EACH	1
705-06.20	TANGENT ENERGY ABSORBING TERM MASH TL-3	EACH	1
705-06.30	GR TERMINAL (ENERGY ABSORBING) MASH TL2	EACH	1
(2)(3)(4) 709-05.05	MACHINED RIP-RAP (CLASS A-3)	TON	100
712-01	TRAFFIC CONTROL	LS	1
712-02.02	INTERCONNECTED PORTABLE BARRIER RAIL	L.F.	370
712-02.60	TEMPORARY WORK ZONE CRASH CUSHION (MASH TL-3)	EACH	4
712-04.01	FLEXIBLE DRUMS (CHANNELIZING)	EACH	60
712-04.50	BARRIER RAIL DELINEATOR	EACH	23
(6) 712-06	SIGNS (CONSTRUCTION)	S.F.	842
712-07.03	TEMPORARY BARRICADES (TYPE III)	L.F.	36
712-09.04	REMOVABLE PAVEMENT MARKING (STOP LINE)	L.F.	35
(16) 713-15	REMOVAL OF SIGNS, POSTS AND FOOTING	LS	1
716-01.21	SNOWPLOWABLE RAISED PAVEMENT MARKERS (BI-DIR) (1 COLOR)	EACH	12
(7) 716-01.30	REMOVAL OF SNOWPLOWABLE REFLECTIVE MARKER	EACH	12
(8) 716-05.20	PAINTED PAVEMENT MARKING (6" LINE)	L.M.	0.6
716-08.20	REMOVAL OF PAVEMENT MARKING (LINE)	L.M.	0.2
(13) 716-12.02	ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE)	L.M.	0.4
717-01	MOBILIZATION	LS	1
(2)(3)(4) 740-10.03	GEOTEXTILE (TYPE III)(EROSION CONTROL)	S.Y.	172
(2)(3) 740-11.02	TEMPORARY SEDIMENT TUBE 12IN	L.F.	80
(2)(3)(14) 801-01.07	TEMPORARY SEEDING (WITH MULCH)	UNIT	5
(2)(3)(14) 801-02	SEEDING (WITHOUT MULCH)	UNIT	10
(2)(3)(9) 801-03	WATER (SEEDING & SODDING)	M.G.	1
(10) 803-01	SODDING (NEW SOD)	S.Y.	1112

FOOTNOTES	
(1)	INCLUDES <u>14 C.Y.</u> FOR TEMPORARY CONSTRUCTION EXITS.
(2)	SEE SUBSECTION 209.07 OF THE STANDARD SPECIFICATIONS FOR MAINTENANCE AND REPLACEMENT.
(3)	ALL QUANTITIES ARE TO BE USED AS DIRECTED BY THE TDOT MANAGER.
(4)	TO BE USED FOR CONSTRUCTION EXITS.
(5)	TO BE USED FOR STRAIGHT CONCRETE ENDWALL.
(6)	FOR PLACEMENT OF CONSTRUCTION SIGNS SEE SHEET TRAFFIC CONTROL SHEETS.
(7)	ANY DAMAGE OCCURRED DURING REMOVAL SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE AND TO THE SATISFACTION OF THE TDOT MANAGER.
(8)	TO BE USED FOR TEMPORARY TRAFFIC CONTROL AND TEMPORARY PAVEMENT MARKING ON INTERMEDIATE LAYERS.
(9)	INCLUDES <u>1 THOUSAND GALLONS</u> FOR EROSION PREVENTION AND SEDIMENT CONTROL.
(10)	INCLUDES <u>19 CY</u> TO BE USED FOR SPECIAL DITCH.
(11)	SEE GRADING SPECIAL NOTES ON SHEET 2D.
(12)	TO BE USED FROM STA. 112+00 TO 113+50 OR AS DIRECTED BY THE TDOT MANAGER.
(13)	THE CONTRACTOR SHALL USE THE EXTRUDED OR RIBBON METHOD FOR APPLICATION.
(14)	THE COST OF FERTILIZER AND LIME USED IN INITIAL SEED BED PREPARATION IS TO BE INCLUDED IN THE COST OF SEEDING. SEE SECTION 801 OF TDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.
(15)	THE BEDDING MATERIAL SHALL BE INCLUDED IN THE COST OF THE PROPOSED PIPE CULVERT.
(16)	THE REMOVED 20 MPH SPEED LIMIT SIGN SHALL BE REPLACED ON N CROSS ST.

TYPE	YEAR	PROJECT NO.	SHEET NO.
PS&E	2025	PROT-29(131)	2

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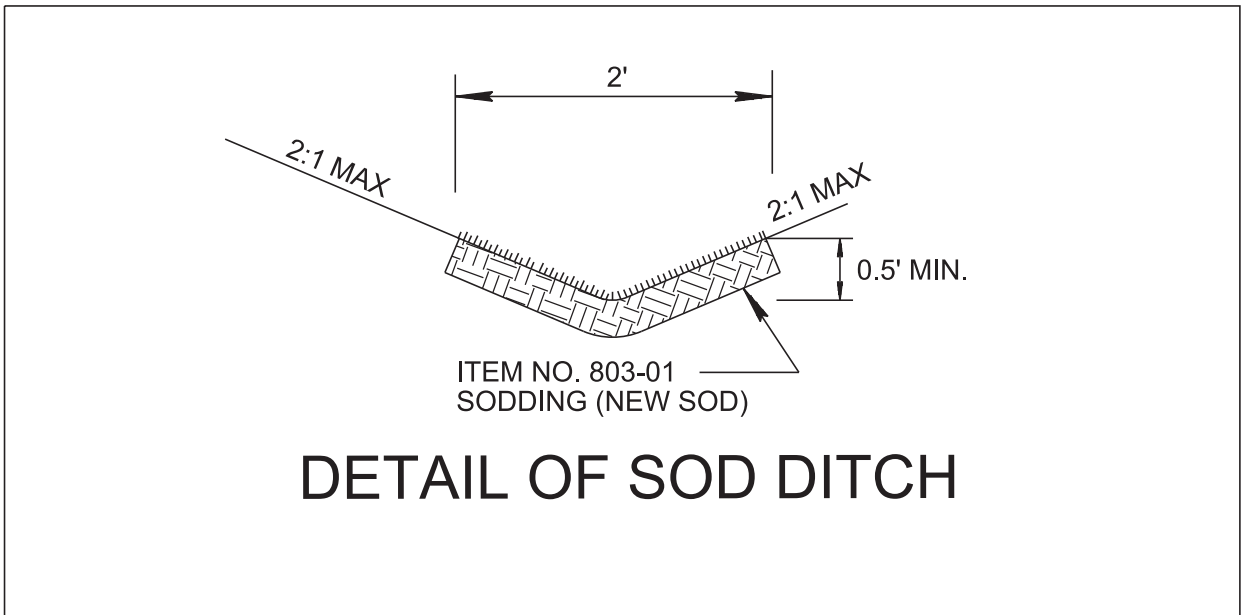
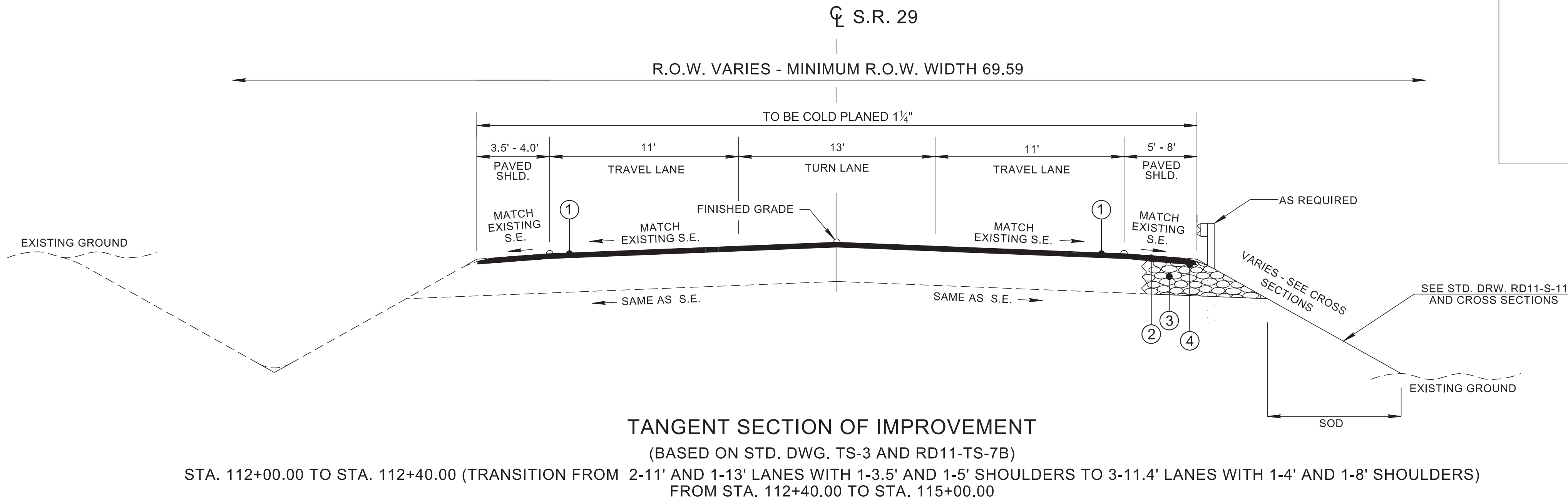
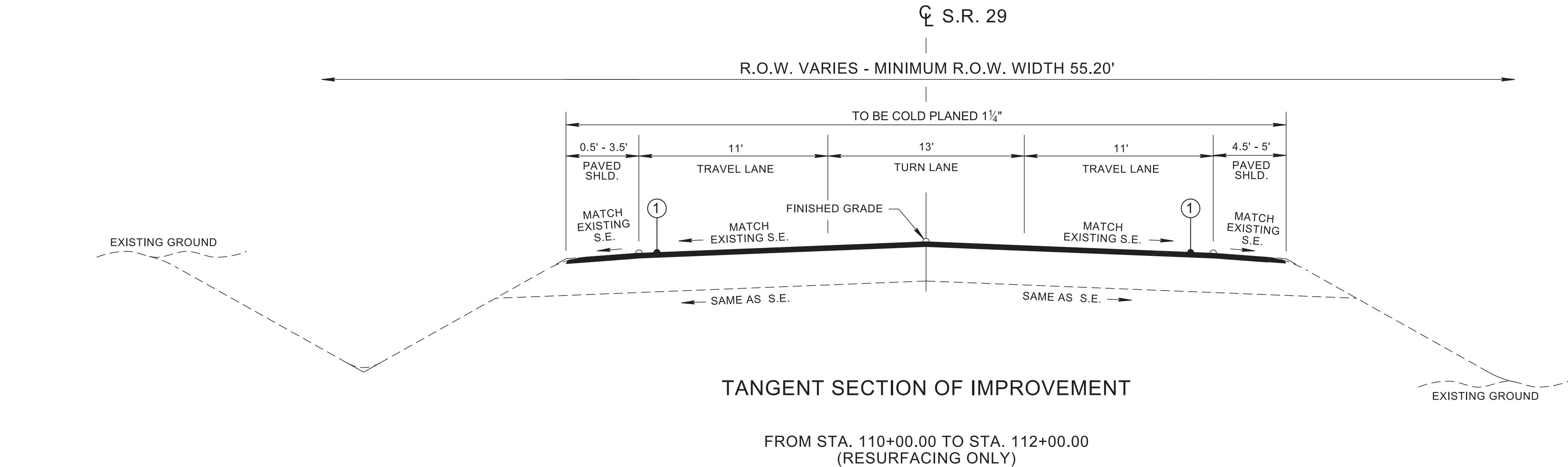


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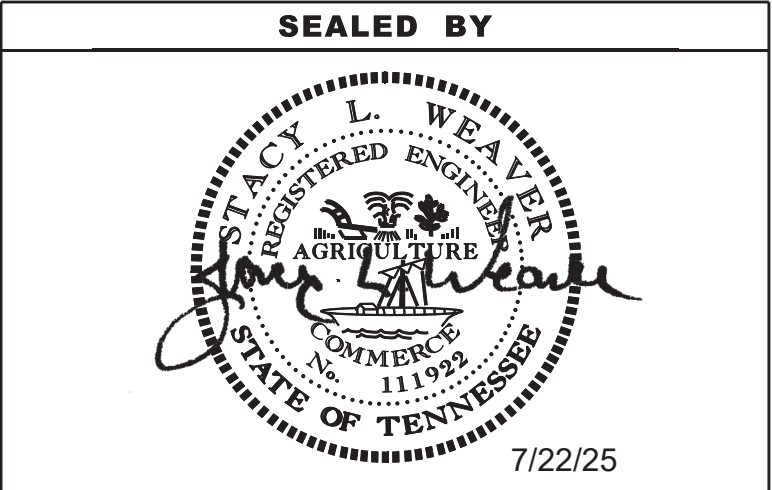
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ESTIMATED
ROADWAY
QUANTITIES

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2022	76S029-M2-003	2B
PS&E	2025	PROT-29(131)	2B



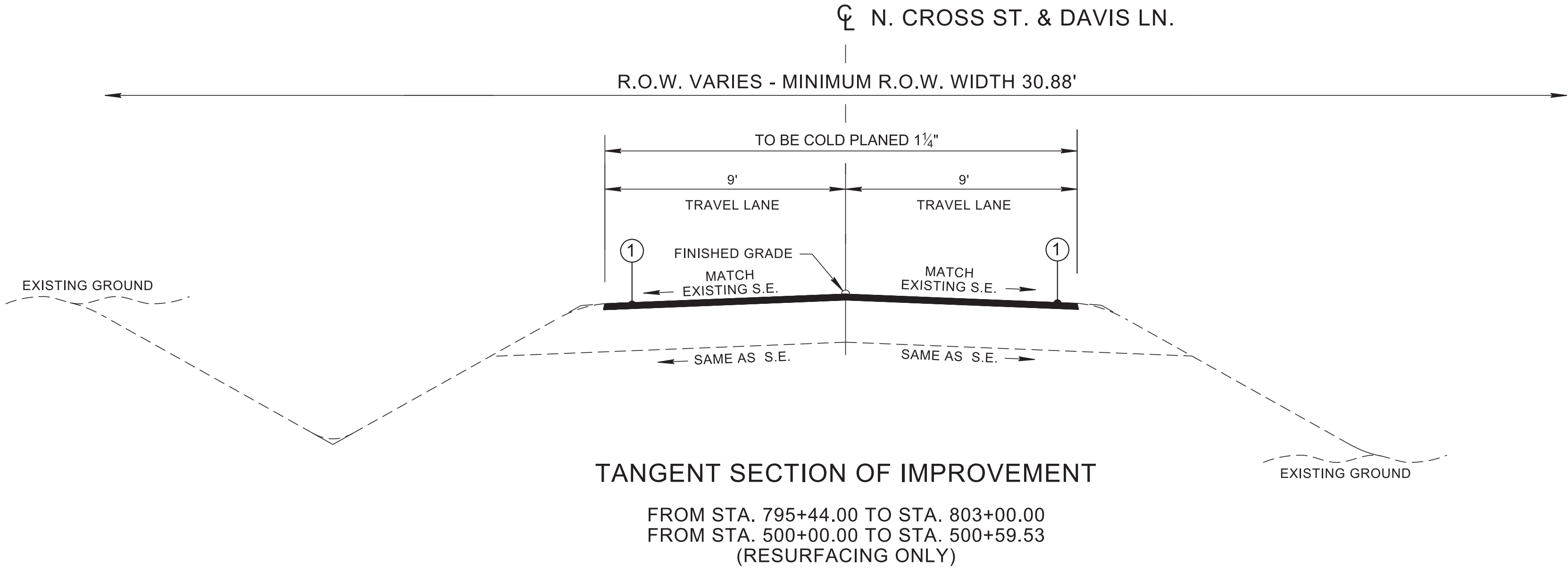
PROPOSED PAVEMENT SCHEDULE	
① ASPHALTIC CONCRETE SURFACE (HOT MIX) PG64-22 GRADING "D" SURFACE @ 1.25"+ THICK (APPROX. 132.5 LB./S.Y.) 411-01.10 ACS MIX (PG64-22) GRADING "D"	③ MINERAL AGGREGATE 16.5"+ THICK 303-01 MINERAL AGGREGATE, TYPE "A" BASE, GRADING "D"
② TACK COAT 403-01 BITUMINOUS MATERIAL FOR TACK COAT (TC) SEE SECTION 403.05 OF THE STANDARD SPECIFICATION FOR DETERMINING APPLICATION RATE IN THE FIELD	④ PRIME COAT 402-01 BITUMINOUS MATERIAL FOR PRIME COAT (PC) AT 0.35 GAL/SQ.YD. 402-02 AGGREGATE FOR COVER MATERIAL (PC) AT 8 – 12 LB./SQ. YD.



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TYPICAL
SECTIONS AND
PAVEMENT
SCHEDULE

TYPE	YEAR	PROJECT NO.	SHEET NO.
PS&E	2025	PROT-29(131)	2B1



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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TYPICAL
SECTIONS

GENERAL NOTES

GRADING

- (1) ANY AREA THAT IS DISTURBED OUTSIDE LIMITS OF CONSTRUCTION DURING THE LIFE OF THIS PROJECT SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.
- (2) CERTIFICATION FOR ALL BORROW PITS MUST BE OBTAINED IN ACCORDANCE WITH SUBSECTION 107.06 OF THE STANDARD SPECIFICATIONS.
- (3) THE CONTRACTOR SHALL NOT DISPOSE OF ANY MATERIAL EITHER ON OR OFF STATE-OWNED R.O.W. IN A REGULATORY FLOOD WAY AS DEFINED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY (FEMA) WITHOUT APPROVAL BY FEMA. ALL MATERIAL SHALL BE DISPOSED OF IN UPLAND (NON-WETLAND) AREAS AND ABOVE ORDINARY HIGH WATER OF ANY ADJACENT WATERCOURSE. THIS DOES NOT ELIMINATE THE NEED TO OBTAIN ANY OTHER LICENSES OR PERMITS THAT MAY BE REQUIRED BY ANY OTHER FEDERAL, STATE OR LOCAL AGENCY.

SEEDING AND SODDING

- (1) ITEM NO. 801-02, SEEDING (WITHOUT MULCH) AND EROSION CONTROL BLANKET, SHALL BE PLACED AT LOCATIONS SHOWN ON THE PLANS AS WELL AS LOCATIONS DIRECTED BY THE ENGINEER.
- (2) SOD SHALL BE PLACED AT LOCATIONS SHOWN ON THE PLANS TO PREVENT DAMAGE TO ADJACENT FACILITIES AND PROPERTY DUE TO EROSION ON ALL NEWLY GRADED CUT AND FILL SLOPES AS WORK PROGRESSES
- (3) ITEM NO. 803-01 SHALL BE USED ON SLOPES 3:1 OR STEEPER AND OTHER AREAS AS INDICATED IN THE PLANS THAT ARE INACCESSIBLE FOR MOWING.

GUARDRAIL

- (1) THE PROPOSED GUARDRAIL, INCLUDING ANY ANCHOR SYSTEM, SHALL BE INSTALLED QUICKLY TO MINIMIZE TRAFFIC EXPOSURE TO ANY HAZARD. NO PAYMENT WILL BE MADE FOR A SECTION OF PROPOSED GUARDRAIL, INCLUDING ANCHORS, UNTIL IT IS COMPLETE IN PLACE.

DRAINAGE

- (1) THE CONTRACTOR SHALL SHAPE DITCHES TO THE SPECIFIED DESIGN. THIS WORK WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COST WILL BE INCLUDED IN THE COST OF OTHER ITEMS.
- (2) EXCAVATION FOR PIPE CULVERT WILL NOT BE MEASURED AND PAID FOR DIRECTLY BUT WILL BE INCLUDED IN THE PRICE BID PER LINEAR FOOT OF PIPE.
- (3) THE CUTTING OF INLET AND OUTLET DITCHES WHERE SHOWN ON PLANS OR AS DIRECTED BY THE ENGINEER WILL BE MEASURED AND PAID FOR AS ITEM NO. 203-01 ROAD AND DRAINAGE EXCAVATION (UNCLASSIFIED).
- (4) WHERE A CULVERT (PIPE) IS MOVED TO A NEW LOCATION OTHER THAN THAT SHOWN ON THE PLANS, INCREASING OR DECREASING THE AMOUNT OF CULVERT EXCAVATION WILL NOT RESULT IN AN INCREASE OR DECREASE IN THE AMOUNT OF PAYMENT THAT WILL BE MADE DUE TO SUCH CHANGE.
- (5) DURING CONSTRUCTION OF DRAINAGE STRUCTURES ALL COST ASSOCIATED WITH MAINTAINING THE FLOW OF WATER AND TRAFFIC, AT THESE STRUCTURES, DURING THE PHASED CONSTRUCTION OF THIS PROJECT ARE TO BE INCLUDED IN THE UNIT PRICE OF THE DRAINAGE STRUCTURES AND TRAFFIC CONTROL ITEMS.

MISCELLANEOUS

- (2) THE CONTRACTOR SHALL BE REQUIRED TO REMOVE AND RESET MAILBOXES AND POSTS WHERE AND AS DIRECTED BY THE ENGINEER. COST TO BE INCLUDED IN PRICE BID FOR OTHER CONSTRUCTION ITEMS.
- (3) NOTHING IN THE GENERAL NOTES OR SPECIAL PROVISIONS SHALL RELIEVE THE CONTRACTOR FROM HIS RESPONSIBILITIES TOWARD THE SAFETY AND CONVENIENCE OF THE GENERAL PUBLIC AND THE RESIDENTS ALONG THE PROPOSED CONSTRUCTION AREA.

ROAD CLOSURE

- (1) NO LESS THAN SEVEN (7) DAYS PRIOR TO THE CLOSURE OF THE ROAD, THE CONTRACTOR SHALL NOTIFY THE FOLLOWING INDIVIDUALS OR AGENCIES COMPLETELY DESCRIBING THE AFFECTED ROADS AND THE APPROXIMATE DURATION OF THE CONSTRUCTION: THESE PARTIES INCLUDE, BUT ARE NOT LIMITED TO: (1) LOCAL LAW ENFORCEMENT OFFICE, (2) LOCAL FIRE DEPARTMENT, (3) AMBULANCE SERVICE, (4) LOCAL SCHOOL SUPERINTENDENT, (5) UNITED STATES POSTAL SERVICE, AND (6) LOCAL ROAD SUPERINTENDENT.

PAVEMENT MARKINGS

TEMPORARY PAVEMENT MARKINGS ON INTERMEDIATE LAYERS

- (1) TEMPORARY PAVEMENT LINE MARKINGS ON INTERMEDIATE LAYERS OF PAVEMENT SHALL BE REFLECTIVE TAPE OR REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT, UNMARKED SECTIONS SHALL NOT BE ALLOWED. THESE MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-05.20, PAINTED PAVEMENT MARKING (6" LINE), L.M.

FINAL PAVEMENT MARKING

- (1) THE CONTRACTOR WILL BE REQUIRED TO PERFORM THE FOLLOWING WORK:

a. BROOMING & DE-GRASSING SHOULDERS SHALL INCLUDE CLIPPING OF MATERIAL INTERFERING WITH PROPER DRAINAGE OF ROADWAY (INCLUDING PAVED AND GRAVEL SHOULDERS), AS DIRECTED BY THE TDOT PROJECT ENGINEER.

b. ALL MATERIAL FROM CLIPPING, BROOMING AND DE-GRASSING SHOULDERS SHALL BE PICKED UP, REMOVED AND PROPERLY DISPOSED AS DIRECTED BY THE TDOT PROJECT ENGINEER.

c. ALL COSTS ASSOCIATED WITH PICKING UP, REMOVAL AND PROPER DISPOSAL SHALL BE PAID FOR UNDER ITEM NO. 208-01.05.

d. REMOVE ALL GARBAGE AND CONSTRUCTION DEBRIS FROM PROJECT. THE COST FOR THIS WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (2) PERMANENT PAVEMENT LINE MARKINGS SHALL BE 6" ENHANCED FLATLINE THERMOPLASTIC INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-12.02, ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE), L.M. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.

DETOURS, LANE SHIFTS AND MEDIAN CROSS-OVERS


- (1) THE PAVEMENT MARKING ON THE LANE SHIFT FOR LANE LINES WILL BE INSTALLED AND MAINTAINED TO THE SAME STANDARDS AS FOR PERMANENT MARKINGS ON THE MAIN ROADWAY. THESE MARKINGS SHALL BE IN PLACE PRIOR TO ALLOWING TRAFFIC ONTO THE PAVEMENT. THESE PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-05.20 PAINTED PAVEMENT MARKING (6" LINE), LIN. L.M.
- (2) BEFORE OPENING THE LANE SHIFT TO TRAFFIC, THE TRANSITIONAL MARKINGS ON THE EXISTING ROADWAY MUST BE IN PLACE. ALL EXISTING MARKINGS IN THE AREA OF THESE TRANSITIONAL MARKINGS SHALL BE OBLITERATED AND ALL EXISTING RAISED PAVEMENT MARKERS SHALL BE REMOVED TO ELIMINATE CONFLICTING MARKINGS. REMOVAL OF THE EXISTING CONFLICTING MARKINGS AND RAISED PAVEMENT MARKERS WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COST WILL BE INCLUDED IN ITEM NO. 712-01, TRAFFIC CONTROL, LUMP SUM.

SNOWPLOWABLE REFLECTIVE PAVEMENT MARKERS

- (1) REMOVE EXISTING SNOWPLOWABLE MARKERS PRIOR TO PAVING AND/OR COLD PLANING. REMOVE ALL ADHESIVES PRIOR TO PAVING. PATCH ANY HOLES OR DIVOTS RESULTING FROM THE REMOVAL OF A MARKER IN A MANNER WHICH ENSURES A UNIFORM PAVED SURFACE. PATCH WORK SHALL BE INCLUDED WITH COST OF OTHER ITEMS OF CONSTRUCTION.

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GENERAL
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GENERAL NOTES (CONT'D)

PAVEMENT

PAVING

- (1) THE CONTRACTOR SHALL BE REQUIRED TO PAVE IN THE DIRECTION OF TRAFFIC.
- (2) THE CONTRACTOR SHALL BE REQUIRED TO COLD PLANE AND PAVE IN THE DIRECTION OF TRAFFIC.
- (3) THE CONTRACTOR SHALL ATTACH A DEVICE TO THE SCREED OF THE PAVER SUCH THAT MATERIAL IS CONFINED AT THE END GATE AND EXTRUDES THE ASPHALT MATERIAL IN SUCH A WAY THAT RESULTS IN A CONSOLIDATED WEDGE-SHAPE PAVEMENT EDGE OF APPROXIMATELY 25 TO 30 DEGREES AS IT LEAVES THE PAVER (MEASURED FROM A LINE PARALLEL TO THE PAVEMENT SURFACE.) THE DEVICE SHALL MEET THE REQUIREMENTS THAT ARE CURRENTLY SET FORTH IN SPECIAL PROVISION 407SE.

CONSTRUCTION WORK ZONE & TRAFFIC CONTROL


- (1) ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.
- (2) IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.
- (3) A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.
- (4) TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.
- (5) USE OF BARRICADES, PORTABLE BARRIER RAILS, AND DRUMS SHALL BE LIMITED TO THE IMMEDIATE AREAS OF CONSTRUCTION WHERE A HAZARD IS PRESENT. THESE DEVICES SHALL NOT BE STORED ALONG THE ROADWAY WITHIN THIRTY (30) FEET OF THE EDGE OF THE TRAVELED WAY BEFORE OR AFTER USE UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL INCREASE TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. THESE DEVICES SHALL BE REMOVED FROM THE CONSTRUCTION WORK ZONE WHEN THE ENGINEER DETERMINES THEY ARE NO LONGER NEEDED. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.
- (6) THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO PARK WITHIN THIRTY (30) FEET OF AN OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.
- (7) ALL DETOUR AND CONSTRUCTION SIGNING SHALL BE IN STRICT ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- (8) THE CONTRACTOR SHALL BE RESPONSIBLE FOR STAKING CONSTRUCTION SIGNS. THE COST OF THIS WORK SHALL BE INCLUDED IN ITEM NO. 712-06, SIGNS (CONSTRUCTION), S.F.

SIGNING

- (1) THE TOP OF THE SIGN FOOTINGS SHALL BE PLACED LEVEL WITH THE GROUND LINE.
- (2) AFTER THE SIGN LOCATIONS HAVE BEEN STAKED, BUT PRIOR TO ORDERING ANY MATERIAL FOR THE SUPPORTS, THERE SHALL BE A FIELD INSPECTION AND APPROVAL BY THE REGIONAL CONSTRUCTION OFFICE
- (3) ALL SIGNS MARKED "TO BE REMOVED" ARE TO BE REMOVED BY THE CONTRACTOR AND PAID FOR UNDER ITEM NO. 713-15 AND BECOME THE PROPERTY OF THE CONTRACTOR.
- (4) THE EXISTING FOOTINGS ARE TO BE REMOVED 6 INCHES BELOW GROUND LINE.
- (5) THE LETTERS, DIGITS, ARROWS, BORDERS, AND ALPHABET ACCESSORIES ON ALL FLAT SHEET SIGNS SHALL BE APPLIED BY SILK SCREENING PROCESS.
- (6) ALL SIGNS WHICH INTERFERE WITH CONSTRUCTION WILL BE RELOCATED OUTSIDE LIMITS OF CONSTRUCTION BY THE CONTRACTOR. UPON COMPLETION OF CONSTRUCTION, THE CONTRACTOR WILL RESTORE THE SIGNS TO ORIGINAL LOCATION. THE CONTRACTOR SHALL CHECK WITH THE REGIONAL TRAFFIC ENGINEER PRIOR TO MOVING ANY PERMANENT SIGNS.

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GENERAL
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SPECIAL NOTES

GRADING

- (1) THE GRADING TABULATIONS AND RESULTING EARTHWORK ASSOCIATED BID QUANTITIES WERE PREPARED UTILIZING AVAILABLE GEOTECHNICAL INFORMATION AND/OR REPORTS PREPARED FOR THIS PROJECT. THIS INFORMATION IS PROVIDED FOR GENERAL INFORMATION AND ESTIMATION GUIDANCE ONLY.
- (2) BORING DEPICTIONS SHOWN ON THE FOUNDATION DATA SHEETS, SOILS SHEETS, PLANS, AND CROSS-SECTIONS INDICATE SOIL AND ROCK CONDITIONS AT THE SPECIFIC BORING LOCATIONS. ANY SOIL PROFILE AND/OR ROCK LINE IS INTERPRETIVE BASED ON THE JUDGMENT OF THE GEOTECHNICAL ENGINEER/GEOLOGIST. THE TRANSITION BETWEEN BORINGS AND LAYERS MAY VARY SIGNIFICANTLY DEPENDING ON THE GEOLOGIC FORMATIONS ENCOUNTERED.
- (4) THE CONTRACTOR SHALL UTILIZE ALL INFORMATION PROVIDED IN THE PLANS, CROSS-SECTIONS AND CONTRACT DOCUMENTS INCLUDING ANY SPECIAL PROVISIONS AS WELL AS UTILIZING HIS PAST EXPERIENCE WITH PROJECTS OF SIMILAR NATURE, SCOPE AND LOCATION IN PREPARATION OF HIS BID FOR EARTHWORK ITEMS. IT IS THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE AND PROVIDE EQUIPMENT AND MEANS NECESSARY TO CONDUCT THE EXCAVATION ACTIVITIES IN ACCORDANCE WITH PLANS AND SPECIFICATIONS.
- (5) EARTHWORK IS PAID FOR UNDER ITEM NO. 203-01, ROAD AND DRAINAGE EXCAVATION (UNCLASSIFIED). NO ADDITIONAL PAYMENT WILL BE MADE FOR EARTHWORK QUANTITIES BASED SOLELY ON A CLAIM THAT THE QUANTITIES SHOWN IN THE GRADING TABULATION OR ELSEWHERE IN THE PLANS ARE INACCURATE WITH RESPECT TO THE TYPE OF MATERIALS ENCOUNTERED DURING CONSTRUCTION EXCEPT AS PROVIDED FOR BY SECTION 104.02 IN THE CURRENT EDITION OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION OR AS AMENDED IN SUPPLEMENTAL SPECIFICATIONS.


DEMOLITION

DEMOLITION OF BUILDINGS

- (1) IF THE ASBESTOS SURVEY AND ABATEMENT IS NOT PART OF THE CONTRACT, THE CONTRACTOR IS RESPONSIBLE FOR COORDINATING WITH THE TDOT HAZARDOUS MATERIALS OFFICE TO VERIFY THAT AN ASBESTOS SURVEY HAS BEEN COMPLETED FOR ANY BUILDING TO BE REMOVED. IN THE CASE THAT NO SURVEY HAS BEEN COMPLETED THE CONTRACTOR SHALL COORDINATE WITH THE HAZARDOUS MATERIAL OFFICE IN SCHEDULING A SURVEY.
- (2) ASBESTOS-CONTAINING MATERIALS (ACM) ABATEMENT SHALL BE COMPLETED PRIOR TO ANY DEMOLITION ACTIVITIES FOR BUILDINGS INCLUDED IN THE PROJECT. ABATEMENT SHOULD BE ACCOMPLISHED PER SP202ACM SPECIAL PROVISION REGARDING REMOVAL OF ASBESTOS-CONTAINING MATERIALS. STATE OF TENNESSEE ASBESTOS ACCREDITATION REQUIREMENTS (TCA 1200-01-20) MANDATE THAT ACM ABATEMENT WORK BE PERFORMED BY AN ACCREDITED FIRM (CONTRACTOR) USING ACCREDITED ABATEMENT WORKERS AND SUPERVISORS.
- (3) THE CONTRACTOR SHALL BE RESPONSIBLE FOR SUBMITTING A NOTICE TO THE TDEC, DIVISION OF AIR POLLUTION CONTROL TEN (10) DAYS IN ADVANCE OF ANY ACM ABATEMENT, DEMOLITION, OR MAJOR REPAIR INVOLVING THE REMOVAL/REPLACEMENT OF A STRUCTURAL COMPONENT.

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ENVIRONMENTAL NOTES

ENVIRONMENTAL GENERAL NOTES

NATURAL RESOURCES

- (1) SOIL MATERIALS MUST BE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. EPSC MEASURES TO PROTECT NATURAL RESOURCES AND WATER QUALITY SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD. APPROPRIATE EPSC MEASURES MUST BE INSTALLED ALONG THE BASE OF ALL FILLS AND CUTS, ON THE DOWNHILL SIDE OF STOCKPILED SOIL, AND ALONG NATURAL RESOURCES IN CLEARED AREAS TO PREVENT SEDIMENT MIGRATION INTO STREAMS, WETLANDS OR OTHER NATURAL FEATURES IN ACCORDANCE WITH TDOT STANDARDS. EPSC MEASURES SHALL BE INSTALLED ON THE CONTOUR, ENTRENCHED AND STAKED, AND EXTEND THE WIDTH OF THE AREA TO BE CLEARED.
- (2) NEW CHANNEL CONSTRUCTION SHALL BE COMPLETED IN THE DRY AND STABILIZED FOR AT LEAST 72 HOURS PRIOR TO DIVERTING WATER FROM THE EXISTING AND/OR TEMPORARY CHANNEL.
- (3) INSTREAM EPSC DEVICES REQUIRE THE TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION REVIEW AND MUST BE PROCESSED BY THE PERMITS SECTION TO OBTAIN WATER QUALITY PERMITS.
- (4) THE OPERATION OF EQUIPMENT IN WATERS OF THE STATE/U.S., INCLUDING WETLANDS AND EPHEMERAL, INTERMITTENT, AND PERENNIAL STREAMS, IS NOT ALLOWED.
- (5) THE WIDTH OF THE FILL ASSOCIATED WITH TEMPORARY CROSSINGS SHALL BE LIMITED TO THE MINIMUM NECESSARY FOR THE ACTUAL CROSSING, NOT TO EXCEED THE WIDTH SPECIFIED IN THE STANDARD DRAWING.
- (6) STREAM BEDS SHALL NOT BE USED AS TRANSPORTATION ROUTES FOR CONSTRUCTION EQUIPMENT. TEMPORARY CULVERT CROSSINGS SHALL BE LIMITED TO ONE POINT PER STREAM AND EPSC MEASURES SHALL BE USED WHERE THE STREAM BANKS ARE DISTURBED. WHERE THE STREAMBED IS NOT COMPOSED OF BEDROCK, A PAD OF CLEAN ROCK SHALL BE USED AT THE CROSSING POINT AND CULVERTED TO PREVENT THE IMPOUNDMENT OF WATER FLOW. CLEAN ROCK IS ROCK OF VARIOUS TYPE AND SIZE, DEPENDING UPON APPLICATION, WHICH CONTAINS NO FINES, SOILS, OR OTHER WASTES OR CONTAMINANTS. OTHER MATERIALS USED FOR ALL TEMPORARY FILLS SHALL BE COMPLETELY REMOVED IN THEIR ENTIRETY AFTER THE WORK IS COMPLETED AND THE AFFECTED AREAS RETURNED TO PREEXISTING ELEVATIONS. ALL TEMPORARY CROSSINGS SHALL BE CONSTRUCTED IN ACCORDANCE WITH STD. DWG. EC-STR-25 UNLESS SPECIFICALLY ADDRESSED IN THE EPSC PLANS. ALTERNATIVELY, PLACING A TEMPORARY BRIDGE (E.G. BAILEY BRIDGE OR EQUIVALENT, TIMBERS, ETC.) FROM TOP OF BANK TO TOP OF BANK OR THE APPROPRIATE USE OF BARGES AT THE CROSSING TO AVOID DISTURBANCE OF THE STREAMBED IS AN ACCEPTABLE OPTION.
- (7) HEAVY EQUIPMENT WORKING IN WETLANDS WITH PERMITTED TEMPORARY IMPACTS SHALL BE PLACED ON MATS, OR OTHER MEASURES MUST BE TAKEN TO MINIMIZE SOIL DISTURBANCE AND COMPACTION UNLESS SPECIFICALLY ADDRESSED IN THE CONSTRUCTION PLANS. ANY MATS AND OTHER MEASURES USED FOR HEAVY EQUIPMENT SHALL BE REMOVED IN THEIR ENTIRETY AFTER THE WORK IS COMPLETED. ALL AFFECTED AREAS SHOULD BE RETURNED TO PRE-EXISTING CONDITIONS.
- (8) WETLANDS SHALL NOT BE USED AS EQUIPMENT STORAGE, STAGING, OR TRANSPORTATION AREAS, UNLESS SPECIFICALLY PROVIDED FOR IN THE CONSTRUCTION PLANS AND PERMITS.
- (9) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS PRIOR TO ANY CONSTRUCTION AND MAINTENANCE ACTIVITIES TO ENSURE THAT ENVIRONMENTAL FEATURES (E.G., STREAMS, WETLANDS, SPRINGS, ETC.) ARE NOT IMPACTED BEYOND PERMITTED LOCATIONS. IF THE CONTRACTOR OR TDOT INSPECTOR IS UNSURE OF THE IDENTITY OF AN ENVIRONMENTAL FEATURE, THE INSPECTOR SHALL CONTACT THE TDOT REGION ENVIRONMENTAL TECH GROUP IMMEDIATELY.

SPECIES

- (10) NO ACTIVITY MAY SUBSTANTIALLY DISRUPT THE MOVEMENT OF THOSE SPECIES OF AQUATIC LIFE INDIGENOUS TO THE WATER BODY, INCLUDING THOSE SPECIES THAT NORMALLY MIGRATE THROUGH THE AREA.
- (11) SHOULD CLIFF SWALLOW OR BARN SWALLOW NESTS, EGGS, OR BIRDS (YOUNG AND ADULTS) BE PRESENT, THE CONTRACTOR SHALL CONTACT THE REGIONAL ECOLOGY OFFICE TO DETERMINE IF SEASONAL RESTRICTIONS WILL BE NECESSARY. GENERALLY, BIRDS, NESTS, AND EGGS MAY NOT BE DISTURBED BETWEEN APRIL 15 AND JULY 31. FROM AUGUST 1 TO APRIL 14, NESTS CAN BE REMOVED OR DESTROYED SO LONG AS BIRDS OR EGGS ARE NOT PRESENT, AND MEASURES IMPLEMENTED TO PREVENT FUTURE NEST BUILDING AT THE SITE (I.E., CLOSING OFF AREA USING NETTING).
- (12) IF THE REMOVAL OF ANY TREES WITH A DIAMETER AT BREAST HEIGHT (DBH) GREATER THAN 3 INCHES IS DEEMED NECESSARY THE TDOT SUPERVISOR SHALL CONTACT THE TDOT ENVIRONMENTAL DIVISION, ECOLOGY SECTION IMMEDIATELY.

PERMITS, PLANS & RECORDS

- (13) THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR AND OBTAIN ANY NECESSARY ENVIRONMENTAL PERMITS OR APPROVALS, INCLUDING BUT NOT LIMITED TO ARCHAEOLOGY, ECOLOGY, HISTORICAL, HAZARDOUS MATERIALS, AIR AND NOISE, TDEC ARAP/401, USACE SECTION 404, TVA SECTION 26A, AND TDEC NPDES PERMITS, FROM FEDERAL, STATE AND/OR LOCAL AGENCIES REGARDING ANY MATERIAL AND STAGING AREAS AND THE OPERATION OF ANY PROJECT-DEDICATED ASPHALT AND/OR CONCRETE PLANTS TO BE USED. ANY SUCH PERMITS SHALL BE SUPPLIED TO THE TDOT PROJECT RESPONSIBLE PARTY PRIOR TO THE USE OF THE PERMITTED AREA(S).
- (14) ANY DISAGREEMENT BETWEEN THE CONSTRUCTION PLANS, THE PROJECT AS CONSTRUCTED, AND THE PERMIT(S) ISSUED FOR THE PROJECT, SHALL BE BROUGHT TO THE ATTENTION OF THE TDOT PROJECT RESPONSIBLE PARTY. THE ENVIRONMENTAL DIVISION, DESIGN DIVISION, AND HEADQUARTERS CONSTRUCTION OFFICE SHALL BE CONTACTED IN THESE INSTANCES AND DECIDE WHICH HAS PRECEDENCE AND WHETHER PERMIT OR PLANS REVISIONS ARE NEEDED. IN GENERAL, PERMIT CONDITIONS WILL PREVAIL.
- (15) IF A CHANGE IN PROJECT SCOPE OCCURS DURING CONSTRUCTION, INCLUDING VALUE ENGINEERING, THE TDOT PERMIT SECTION SHALL BE CONTACTED TO DETERMINE WHETHER PERMIT REVISIONS ARE NEEDED. THE ROADWAY DESIGN DIVISION SHALL BE CONTACTED TO DETERMINE IF ANY PLAN REVISIONS ARE NEEDED.
- (16) THE CONTRACTOR SHALL REVIEW ALL EXISTING PERMITS TO ENSURE THAT WORK AT PERMITTED SITES DOES NOT EXCEED EXPIRATION DATE. IF WORK IS GOING TO BE CONTINUED AFTER EXPIRATION DATES, THE CONTRACTOR SHALL CONTACT THE TDOT PROJECT RESPONSIBLE PARTY TO COMMENCE PERMIT RENEWAL PROCESS.
- (17) ALL WATER QUALITY PERMITS SHALL BE POSTED NEAR THE MAIN ENTRANCE OF THE CONSTRUCTION SITE ACCESSIBLE TO THE PUBLIC. THE NAME, COMPANY NAME, EMAIL ADDRESS, TELEPHONE NUMBER AND ADDRESS OF THE PROJECT SITE OWNER, OPERATOR, OR A LOCAL CONTACT PERSON WITH A BRIEF DESCRIPTION OF THE PROJECT SHALL ALSO BE POSTED. IF POSTING THIS INFORMATION NEAR A MAIN ENTRANCE IS INFEASIBLE, THE INFORMATION SHALL BE PLACED IN A PUBLICLY ACCESSIBLE LOCATION NEAR WHERE THE CONSTRUCTION IS ACTIVELY UNDERWAY AND MOVED AS NECESSARY. THIS LOCATION SHALL BE POSTED AT THE CONSTRUCTION SITE. ALL POSTINGS SHALL BE MAINTAINED IN LEGIBLE CONDITION.

SUPPORT ACTIVITIES

- (18) MATERIALS AND STAGING AREAS SHALL NOT AFFECT ANY WATERS OF THE STATE/U.S. UNLESS THESE AREAS ARE SPECIFICALLY COVERED BY ENVIRONMENTAL PERMITS, OBTAINED SOLELY BY THE CONTRACTOR. THE CONTRACTOR SHALL REVIEW ALL EXISTING PERMITS TO ENSURE THAT WORK AT PERMITTED SITES DOES NOT EXCEED EXPIRATION DATES. IF WORK IS GOING TO BE CONTINUED AFTER EXPIRATION DATES, THE CONTRACTOR SHALL CONTACT THE TDOT PROJECT RESPONSIBLE PARTY TO COMMENCE PERMIT RENEWAL PROCESS.

ENVIRONMENTAL

- (19) EXCEPT AS OTHERWISE SPECIFIED, THERE ARE NO KNOWN SPECIAL ENVIRONMENTAL FACTORS PRESENT ON THIS PROJECT THAT INDICATE A NEED FOR SEASONAL LIMITATIONS ON THE CLEARING, GRUBBING, EXCAVATION, GRADING, CUTTING OR FILLING OPERATIONS OR ON THE TOTAL AREA OF EXPOSED SOIL.

ENVIRONMENTAL SPECIAL NOTES

ENVIRONMENTAL

- (1) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION COMPLIANCE AND FIELD SERVICES OFFICE SHALL BE INVITED TO ALL PRE-CONSTRUCTION MEETINGS.

ECOLOGY


- (2) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE SHALL ADVISE THE CONTRACTOR DURING THE PRE-CONSTRUCTION MEETING WHEN ENVIRONMENTAL DIVISION PERSONNEL OR A DESIGNATED CONSULTANT WILL NEED TO BE ONSITE FOR WORK BEING DONE WHICH COULD AFFECT WATERS OF THE STATE/U.S. OR SPECIES.
- (3) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE SHALL ATTEND THE PRE-CONSTRUCTION MEETING FOR ALL PROJECTS WHICH HAVE THREATENED OR ENDANGERED SPECIES OR CRITICAL HABITAT PROXIMAL TO SCHEDULED WORK. THIS WILL PROVIDE THE OPPORTUNITY TO ENSURE THAT PERSONNEL INCLUDING THE CONTRACTOR'S PERSONNEL AND SUBCONTRACTORS ARE MADE AWARE OF THE NECESSARY PRECAUTIONS THAT MUST BE FOLLOWED.
- (4) ALL PROJECTS WITH LEGALLY PROTECTED SPECIES OR CRITICAL HABITAT IDENTIFIED SHALL HAVE MEASURES IN PLACE TO CONTAIN CONCRETE DUST, CEMENT DUST AND ALL OTHER MATERIALS. THESE MATERIALS ARE NOT ALLOWED TO ENTER WATERS OF THE STATE/U.S.

SCOPE OF WORK

- (5) THIS PROJECT INCLUDES REPAIRING AND REBUILDING FILL SLOPES OF S.R. 29 IN THE CITY OF ONEIDA TO MITIGATE FUTURE ROADWAY FAILURES AND WIDENING THE EXISTING S.R. 29 NORTHBOUND SHOULDER. IT INCLUDES REMOVAL OF THE REMAINS OF BURNED-DOWN BUILDING, AND THE EXISTING BUILDING FOUND ALL WITHIN TRACT 3. IN ADDITION, CONTRACTOR SHALL USE THE EXISTING BUILDING BASEMENT WALL AS PART OF THE SHORING SYSTEM. PARTIAL REMOVAL OF THE WALL MAY BE REQUIRED TO ACHIEVE PROPER COMPACTION IN THE FILL AREA. SHORING SHALL BE INSTALLED APPROXIMATELY BETWEEN STA. 112+00 AND STA. 113+50, AS DIRECTED BY THE TDOT MANAGER.
- (6) THIS PROJECT INCLUDES EXTENSION OF THE EXISTING DRAINAGE PIPE AS INDICATED ON THE PLANS, PROFILES, AND CROSS-SECTIONS SHEETS OR AS DIRECTED BY THE TDOT MANAGER.
- (7) THIS PROJECT ALSO INCLUDES INSTALLATION OF PAVEMENT MARKINGS, SODDING, TRAFFIC CONTROL DEVICES, EPSC DEVICES, AND OTHER ITEMS AS INDICATED IN THE PLANS OR AS DIRECTED BY THE TDOT MANAGER.

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ENVIRONMENTAL
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TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2022	76S029-M2-003	2F
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ESTIMATED GRADING QUANTITIES						
DESCRIPTION	UNADJUSTED VOLUMES (CY)		ADJUSTED VOLUMES (CY)	BALANCE SUMMARY		
	EXC.	EMB.	EXC.	SHRINK = 20 % SWELL = 20 %		
MAINLINE	209	0	168	EXC.	EMB.	
SIDE ROADS	0	0				
PVT. DRIVES, BUSINESS AND FIELD ENTRANCES	0	0				
INDEPENDENT DITCHES	0	0				
TEMPORARY CONSTRUCTION EXITS	0	0				
OTHER	0	0				
TOPSOIL (EMB.)	0					
TOPSOIL (EXC.)	0					
TOPSOIL TOTALS (SEE TOPSOIL TABLE)				168	VS.	0
				AVAILABLE	=	168
						</

ESTIMATED GRADING QUANTITIES						
DESCRIPTION	UNADJUSTED VOLUMES (CY)		ADJUSTED VOLUMES (CY)	BALANCE SUMMARY		
	EXC.	EMB.	EXC.	SHRINK = 20 % SWELL = 20 %		
MAINLINE	0	1821		EMB. EXC. 1821 VS. 0 AVAILABLE = 1821 BORROW MATERIAL = 2186		
SIDE ROADS	0	0				
PVT. DRIVES, BUSINESS AND FIELD ENTRANCES	0	0				
INDEPENDENT DITCHES	0	0				
TEMPORARY CONSTRUCTION EXITS	0	0				
OTHER (BRIDGE EXCAVATION, PAVEMENT, ETC...)	0	0				
TOPSOIL (EMB.)	0					
TOPSOIL (EXC.)	0					
TOPSOIL TOTALS (SEE TOPSOIL TABLE)				BORROW MATERIAL = 2186		
ROCK (C.Y.)		TOTALS (C.Y.)				
EXC.	EMB.	EXC. (UNCL.)	EMB. (UNCL.)	EXC (COMMON)	EXC. (AVAIL.)	EXC. (ADJ.)
0	0	0	1821	0	0	0

NOTE: THE CONTRACTOR SHALL MAKE PROVISIONS FOR THE SATISFACTORY DISPOSAL OF 202 C.Y. OF WASTE MATERIAL.

CROSS DRAIN TABULATION																		
STATION	SKEW	RCP CLASS III OR CMP 12 GA OR PVC OR SRTRP OR HDPE OR PP FILL HEIGHT ≤ 16 FT. (L.F.)						RCP CLASS III OR CMP 12 GA OR PVC FILL HEIGHT > 16 FT. AND ≤ 24 FT (L.F.)						END TREATMENT				REMARKS
														INLET		OUTLET		
		18"	24"	30"	36"	42"	48"	18"	24"	30"	36"	42"	48"	TYPE	DRAWING NO.	TYPE	DRAWING NO.	
113+62.27	84°	25														ST	D-PE-4	EXTENSION
TOTALS		25												PIPE TABULATION FOR ARTERIALS ROADWAYS				


PAVEMENT QUANTITIES													
LOCATION (ROADWAY)	TYPE - GRADE - PAY ITEM (TON)												
	MINERAL AGG.	BITUMINOUS PLANT MIX BASE (HOT MIX)				PRIME COAT		TACK COAT	ASPHALTIC CONCRETE SURFACE (HOT MIX)				
		D	A	A-S	B-M				B-M2	D	E		
		303-01	307- 01.01	307- 01.20	307- 01.07				307- 01.08	402-01	402-02	403-01	411- 01.10
S.R.29	260					4	4	2	169				
N. CROSS ST.								1	121				
TOTALS	260					4	4	3	290				

MANHOLES												
SHEET NO.	LOCATION	STATION	OFFSET (FT.)	DRAINAGE CODE	GRATE/ TOP ELEV.	INVERT ELEV.	DEPTH (FT.)	INSIDE DIM. (IN.)	STANDARD DRAWING NO.	PAY ITEMS		
										TYPE 3 611-01-01 0' - 4'	TYPE 3 611-01-02 4' - 8'	TYPE 38 611-38.03 8'-12'
4B	SR 29	113+62.27	22.02	1	1487.73	1480.43	7.3	48	D-MH-2		1	
TOTALS											1	

SPECIAL DITCHES											
ROADWAY	STATION		SIDE	DETAIL NO.	CONFIGURATION			CLASS A CONC. 604-01.01 (C.Y.)	SEEDING WITH MULCH 801-01 (UNITS)	SEEDING W/O MULCH 801-02 (UNITS)	SODDING NEW SOD 803-01 (S.Y.)
	FROM	TO			FORE (H/V)	BOTTOM WIDTH (FT.)	BACK (H/V)				
N. CROSS ST.	113+66.12	114+30.00	RT.		2	0	2				19
TOTALS											19

CROSS DRAIN ENDWALLS								
LOCATION \ SHEET NO.	STATION	OFFSET (FT.)	SKEW	CODE	TYPE	STANDARD DRAWING NO.	STRAIGHT ENDWALLS	
							CLASS "A" CONCRETE 611-07.01 (C.Y.)	STEEL BAR REINFORCING 611-07.02 (LB.)
SR 29 \ 4B	133+66.12	43.15	84°	1	STRAIGHT	D-PE-4	1	45
TOTALS							1	45

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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TABULATED QUANTITIES

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TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2022	76S029-M2-003	2F1
PS&E	2025	PROT-29(131)	2F1

TOPSOIL							
IF EXISTING TOPSOIL IS SUITABLE FOR REUSE							
PROPOSED SLOPE AREA S.F.	EXISTING TOPSOIL (EXC.)	EXISTING TOPSOIL (EMB.)	EXISTING TOPSOIL (TOTAL) C.Y.	REQUIRED TOPSOIL C.Y.	PLACING TOPSOIL 203-04 C.Y.	FURNISHED TOPSOIL 203-07 C.Y.	EXCESS TOPSOIL C.Y.
0	0	0	0	0	0	0	0
IF EXISTING TOPSOIL IS NOT SUITABLE FOR REUSE							
PROPOSED SLOPE AREA S.F.	EXISTING TOPSOIL (EXC.)	EXISTING TOPSOIL (EMB.)	EXISTING TOPSOIL (TOTAL) C.Y.	REQUIRED TOPSOIL C.Y.	PLACING TOPSOIL 203-04 C.Y.	FURNISHED TOPSOIL 203-07 C.Y.	EXCESS TOPSOIL C.Y.
7776	N/A	N/A	N/A	144	N/A	144	N/A

PROPOSED GUARDRAIL

SHEET NO.	LOCATION	SIDE		STATIONS		GUARDRAIL							TERMINAL ANCHORS					REMARKS	
						THRIE BEAM BRIDGE TRAN. MASH TL-3 (20.65') 705-06.25 EACH	METAL BEAM GUARD FENCE 705-01.04 (L.F.)	THRIE BEAM BRIDGE TRAN. MASH TL-2 (20.65') 705-06.26 EACH	MEDIAN DIVIDER		W BEAM GR (TYPE 2) MASH TL3 705-06.01 (L.F.)	MICHIGAN END SHOE 706-10.80 (EACH)	ROUND END ELEMENT 706-10.26 (EACH)	TYPE 12 VARIES 705-04.02 (EACH)	TYPE 13 MASH TL3 (9.375') 705-06.10 (EACH)	SHORT-RADIUS MASH TL3 8' RADIUS 705-06.14 (EACH)	TYPE 38 MASH TL3 (46.875') 705-06.20 (EACH)		TYPE 21 MASH TL2 (21.875') 705-06.30 (EACH)
									WITH	AT CONC.									
		RUB-RAIL (TYPE 2)	BARRIER (26.896') 705-03.04 (L.F.)																
4B	SR 29		X	112+57.81	114+34.74					130					1	1			
4B	N CROSS ST.	X		802+36.60	801+77.77					195						1			
TOTALS										325				1	1	1			

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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TABULATED
QUANTITIES

RIGHT-OF-WAY

- (1)

IT IS INTENDED THAT ALL BUILDINGS AND/OR PORTIONS OF BUILDINGS THAT ARE WITHIN THE PROPOSED RIGHT-OF-WAY AND/OR EASEMENT LINES FOR THE PROJECT BE REMOVED THERE FROM IN THE PROCESS OF RIGHT-OF-WAY ACQUISITION. IF ANY SUCH BUILDINGS OR IMPROVEMENTS ARE NOT REMOVED IN THE COURSE OF RIGHT-OF-WAY ACQUISITION, THE CIVIL ENGINEERING MANAGER 2, PROJECT DEVELOPMENT DIVISION AND THE CIVIL ENGINEERING MANAGER 1, REGIONAL PROJECT DEVELOPMENT OFFICE, ARE TO BE NOTIFIED IN SUFFICIENT TIME TO PERMIT HAVING SUCH REMOVALS DESIGNATED AS A PART OF THE CONSTRUCTION CONTRACT.
- (2)

EXISTING PAVED DRIVEWAY PER TRACT REMAINDER WILL BE REPLACED IN KIND TO A TOUCHDOWN POINT.
- (3)

WHERE THE EXISTING DRIVEWAY IS UNPAVED AND THE PROPOSED DRIVEWAY EXCEEDS 7 PERCENT IN GRADE, EACH DRIVEWAY WILL BE PAVED TO A TOUCHDOWN POINT OR UNTIL THE GRADE IS LESS THAN 7 PERCENT.
- (4)

WHERE THE EXISTING DRIVEWAY IS UNPAVED AND THE PROPOSED DRIVEWAY IS LESS THAN 7 PERCENT IN GRADE, EACH DRIVEWAY WILL BE PAVED A SHOULDER WIDTH FROM THE EDGE OF PAVEMENT AND THE REMAINDER OF THAT DRIVEWAY REPLACED IN KIND TO A TOUCHDOWN POINT.
- (5)

ANY NECESSARY PAVING OF DRIVEWAYS WILL BE DONE DURING PAVING OPERATIONS ON THE MAIN ROADWAY.
- (6)

NEW DRIVEWAYS PROVIDED IN THE PLANS WILL BE PAVED BASED ON THE 7 PERCENT CRITERIA. THOSE 7 PERCENT OR STEEPER IN GRADE WILL BE PAVED AND THOSE FLATTER THAN 7 PERCENT WILL BE COVERED WITH BASE STONE.
- (7)

ON NON-STATE ROUTES, ADDITIONAL DRIVEWAYS AND FIELD ENTRANCES OTHER THAN THOSE PROVIDED IN THE PLANS SHALL REQUIRE A PERMIT ONLY IF THE LOCAL AGENCY SPECIFIES THE NEED FOR THAT PERMIT.
- (8)

ON PROJECTS WITHOUT CURB AND GUTTER THAT ARE ON STATE ROUTES, IT WILL BE THE RESPONSIBILITY OF THE OWNER TO SECURE A PERMIT AND TO CONSTRUCT ADDITIONAL DRIVEWAYS AND FIELD ENTRANCES OTHER THAN THOSE PROVIDED IN THE PLANS.

UTILITY

- (1)

THE LOCATIONS OF UTILITIES SHOWN WITHIN THESE PLANS ARE APPROXIMATE ONLY. THE SURVEYOR HAS NOT PHYSICALLY LOCATED THE UNDERGROUND UTILITIES. ABOVE GRADE AND UNDERGROUND UTILITIES SHOWN WERE TAKEN FROM VISIBLE APPURTENANCES AT THE SITE, PUBLIC RECORDS, AND/OR MAPS PREPARED BY OTHERS. THEREFORE, RELIANCE UPON THE TYPE, SIZE, AND LOCATION OF UTILITIES SHOWN SHOULD BE DONE SO WITH THIS CIRCUMSTANCE CONSIDERED. DETAILED VERIFICATION OF EXISTENCE, LOCATION, AND DEPTH SHOULD ALSO BE MADE PRIOR TO ANY DECISION RELATIVE THERETO IS MADE. AVAILABILITY AND COST OF SERVICE SHOULD BE CONFIRMED WITH THE APPROPRIATE UTILITY COMPANY. IN TENNESSEE, IT IS A REQUIREMENT, PER “THE UNDERGROUND UTILITY DAMAGE PREVENTION ACT”, THAT ANYONE WHO ENGAGES IN EXCAVATION MUST NOTIFY ALL KNOWN UNDERGROUND UTILITY OWNERS, NO LESS THAN THREE (3) OR NOT MORE THAN TEN (10) WORKING DAYS PRIOR TO THE DATE OF THEIR INTENT TO EXCAVATE AND ALSO TO AVOID ANY POSSIBLE HAZARD OR CONFLICT. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC., AT 1-800-351-1111 AS REQUIRED BY TCA 65-31-106 WILL BE REQUIRED.
- (2)

UNLESS OTHERWISE NOTED, ALL UTILITY ADJUSTMENTS WILL BE PERFORMED BY THE UTILITY OR ITS REPRESENTATIVE. THE CONTRACTOR AND UTILITY OWNERS WILL BE REQUIRED TO COOPERATE WITH EACH OTHER IN ORDER TO EXPEDITE THE WORK REQUIRED BY THIS CONTRACT. ON CONTRACTS WHERE CONSTRUCTION STAKES, LINES, AND GRADES ARE CONTRACT ITEMS, THE CONTRACTOR WILL BE REQUIRED TO PROVIDE RIGHT-OF-WAY OR SLOPE STAKES, DITCH OR STREAM BED GRADES, OR OTHER ESSENTIAL SURVEY STAKING TO PREVENT CONFLICTS WITH THE HIGHWAY CONSTRUCTION. FREQUENTLY, THIS WILL BE REQUIRED AS THE FIRST ITEM OF WORK AND AT ANY LOCATION ON THE PROJECT DIRECTED BY THE ENGINEER.
- (3)

THE CONTRACTOR WILL PROVIDE ALL NECESSARY PROTECTIVE MEASURES TO SAFEGUARD EXISTING UTILITIES FROM DAMAGE DURING CONSTRUCTION OF THIS PROJECT. IN THE EVENT THAT SPECIAL EQUIPMENT IS REQUIRED TO WORK OVER AND AROUND THE UTILITIES, THE CONTRACTOR WILL BE REQUIRED TO FURNISH SUCH EQUIPMENT. THE COST OF PROTECTING UTILITIES FROM DAMAGE AND FURNISHING SPECIAL EQUIPMENT WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (4)

PRIOR TO SUBMITTING HIS BID, THE CONTRACTOR WILL BE SOLELY RESPONSIBLE FOR CONTACTING OWNERS OF ALL AFFECTED UTILITIES IN ORDER TO DETERMINE THE EXTENT TO WHICH UTILITY RELOCATIONS AND/OR ADJUSTMENTS WILL HAVE UPON THE SCHEDULE OF WORK FOR THE PROJECT. WHILE SOME WORK MAY BE REQUIRED 'AROUND' UTILITY FACILITIES THAT WILL REMAIN IN PLACE, OTHER UTILITY FACILITIES MAY NEED TO BE ADJUSTED CONCURRENTLY WITH THE CONTRACTOR'S OPERATIONS. ADVANCE CLEAR CUTTING MAY BE REQUIRED BY THE ENGINEER AT ANY LOCATION WHERE CLEARING IS CALLED FOR IN THE SPECIFICATIONS AND CLEAR CUTTING IS NECESSARY FOR A UTILITY RELOCATION. ANY ADDITIONAL COST WILL BE INCLUDED IN THE UNIT PRICE BID FOR THE CLEARING ITEM SPECIFIED IN THE PLANS.
- (5)

THE CONTRACTOR SHALL NOTIFY EACH INDIVIDUAL UTILITY OWNER OF HIS PLAN OF OPERATION IN THE AREA OF THE UTILITIES. PRIOR TO COMMENCING WORK, THE CONTRACTOR SHALL CONTACT THE UTILITY OWNERS AND REQUEST THEM TO PROPERLY LOCATE THEIR RESPECTIVE UTILITY ON THE GROUND. THIS NOTIFICATION SHALL BE GIVEN AT LEAST THREE (3) BUSINESS DAYS PRIOR TO COMMENCEMENT OF OPERATIONS AROUND THE UTILITY IN ACCORDANCE WITH TCA 65-31-106. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC AT 1-800-351-1111 WILL BE REQUIRED.

UTILITY OWNERS

CABLE:
XFINITY (COMCAST)
5720 ASHEVILLE HWY.
KNOXVILLE, TN 37924
CONTACT: JASON KELLY
CELL PHONE: (865) 437-0295
ELECTRIC:
PLATEAU ELECTRIC COOPERATIVE
P.O. BOX 4669
16200 SCOTT HWY.
ONEIDA, TN 37841
OFFICE PHONE: (423) 569-8591
CONTACT: JOEL MCCARTT
GAS:
CITIZENS GAS UTILITY DISTRICT
P.O. BOX 320
12519 SCOTT HIGHWAY
HELENWOOD, TN 37755
OFFICE PHONE: (423) 569-4457
CONTACT: GREG BELL
WATER/SEWER:
ONEIDA WATER & WASTEWATER
19922 ALBERTA STREET
ONEIDA, TN 37841
OFFICE PHONE: (423) 569-6311
FAX: (423) 569-2471
CONTACT: STEPHEN OWENS
CELL PHONE: (423) 539-2322
EMAIL: ONEIDASOWENS@GMAIL.COM
TELEPHONE:
HIGHLAND TELEPHONE COOPERATIVE
P.O.BOX 119
7840 MORGAN COUNTY HWY
SUNBRIGHT, TN 37872
OFFICE PHONE: (423) 628-2121
CONTACT: STEVE GARRETT

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2022	76S029-M2-003	3
PS&E	2025	PROT-29(131)	3

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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

RIGHT-OF-WAY NOTES,
UTILITY NOTES,
AND
UTILITY OWNERS

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TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2022	76S029-M2-003	3A
PS&E	2025	PROT-29(131)	3A

R.O.W. ACQUISITION TABLE

TRACT NO.	PROPERTY OWNERS	COUNTY RECORDS				TOTAL AREA (ACRES)			AREA TO BE ACQUIRED (ACRES)			AREA REMAINING (ACRES)		EASEMENT (ACRES)			
		TAX MAP NO.	PARCEL NO.	DEED DOCUMENT REFERENCE		LEFT	RIGHT	TOTAL	LEFT	RIGHT	TOTAL	LEFT	RIGHT	PERMANENT	SLOPE	CONSTRUCTION	AIR RIGHTS
				BOOK	PAGE												
1	PATRICIA R. MINOR	052H/E	2	37	184		0.330	0.330					0.330				
2	EUGENE AND MAE OLMSTEAD	052H/F	15	186	907	0.994		0.994				0.994					
3	MARY ANN DELK FROMAN, JIMMY BRUCE DELK, PHYLLIS D. DELK JOHNSON, ERIC WAYNE DELK, AND JOSHUA CORY SIBLEY (WITH LIFE ESTATE JESSE DELK, JESSIE NELL DELK SIBLEY AND HUSBAND, JOSEPH C.SIBLEY)	052H/E	1	202	679		0.194	0.194		0.194	0.194		0.000				
4	ANNA LOU STEPHENS	052H/F	16	202	26	0.269		0.269				0.269					
5	TIMOTHY WAYNE WEST	052H/F	17	213	443	0.265		0.265				0.265					
6	BERNARD TERRY	052H/F	18	295	182	0.442		0.442				0.442					
7	TRACOBI, LLC	052H/B	9	218	8		0.619	0.619					0.619				
8	FELICIA GATES, ET AL	052H/A	17	293	298	0.716		0.716				0.716					
9	DARRELL DUNCAN, AND WIFE, CONNIE DUNCAN	052H/D	7	293	320		0.153	0.153					0.153				
10	LESSIE CHAMBERS AND DAUGHTER, MARTHA LOU CHAMBERS	052H/D	6	181	469		0.109	0.109					0.109				
11	MELISSA ANN CHAMBERS	052H/D	5	298	391		0.125	0.125					0.125				
12	JAMES SCOT BOWLING	052H/D	5.01	259	449		0.113	0.113					0.113				
13	J.J. DOWER AND WIFE, TRACIA A. DOWER	052H/D	4.01	246	138		0.133	0.133					0.133				
14	ROBERT WRIGHT AND AMY WRIGHT	052H/B	11.01	165	452		0.313	0.313					0.313				
ACQUISITION TOTALS (ACRES)									0.194								

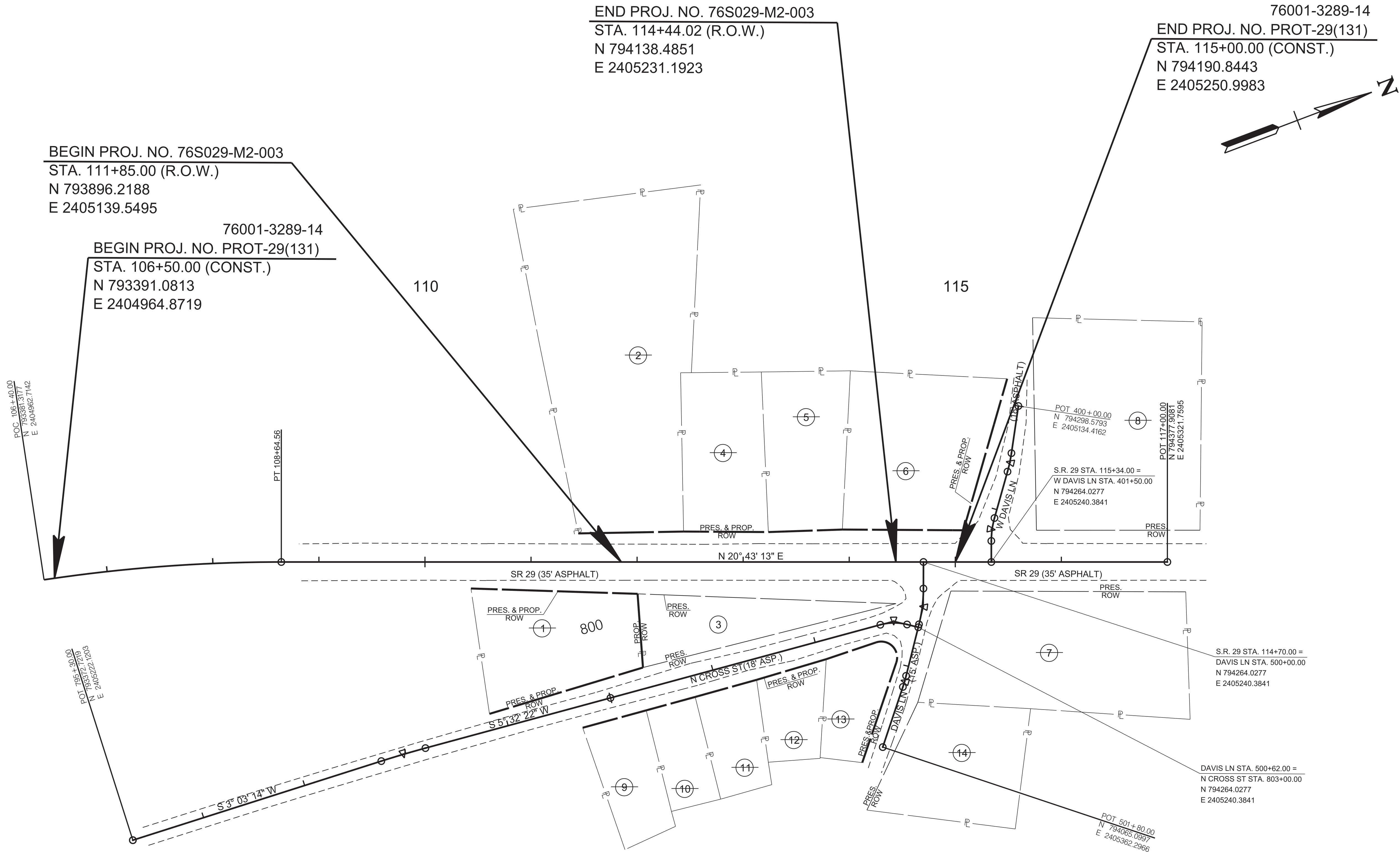
DISTURBED AREA	
IN BETWEEN SLOPE LINES	0.245 (AC)
15 FOOT WIDE STRIP (OUT SIDE SLOPE LINES)	0.127 (AC)
TOTAL DISTURBED AREA	0.372 (AC)
TOTAL PROJECT AREA	0.827 (AC)

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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

RIGHT-OF-WAY
ACQUISITION
TABLE



TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2022	76S029-M2-003	3B
PS&E	2025	PROT-29(131)	3B

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COORDINATES ARE NAD 83(2011), ARE DATUM ADJUSTED BY THE FACTOR OF 1.00005 AND TIED TO THE TGRN. ALL ELEVATIONS ARE REFERENCED TO THE NAVD 1988 WITH GEOID 18.

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PROPERTY
MAP

STA.106+50.00 TO STA.115+00.00
SCALE: 1"=50'

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2022	76S029-M2-003	4
PS&E	2025	PROT-29(131)	4

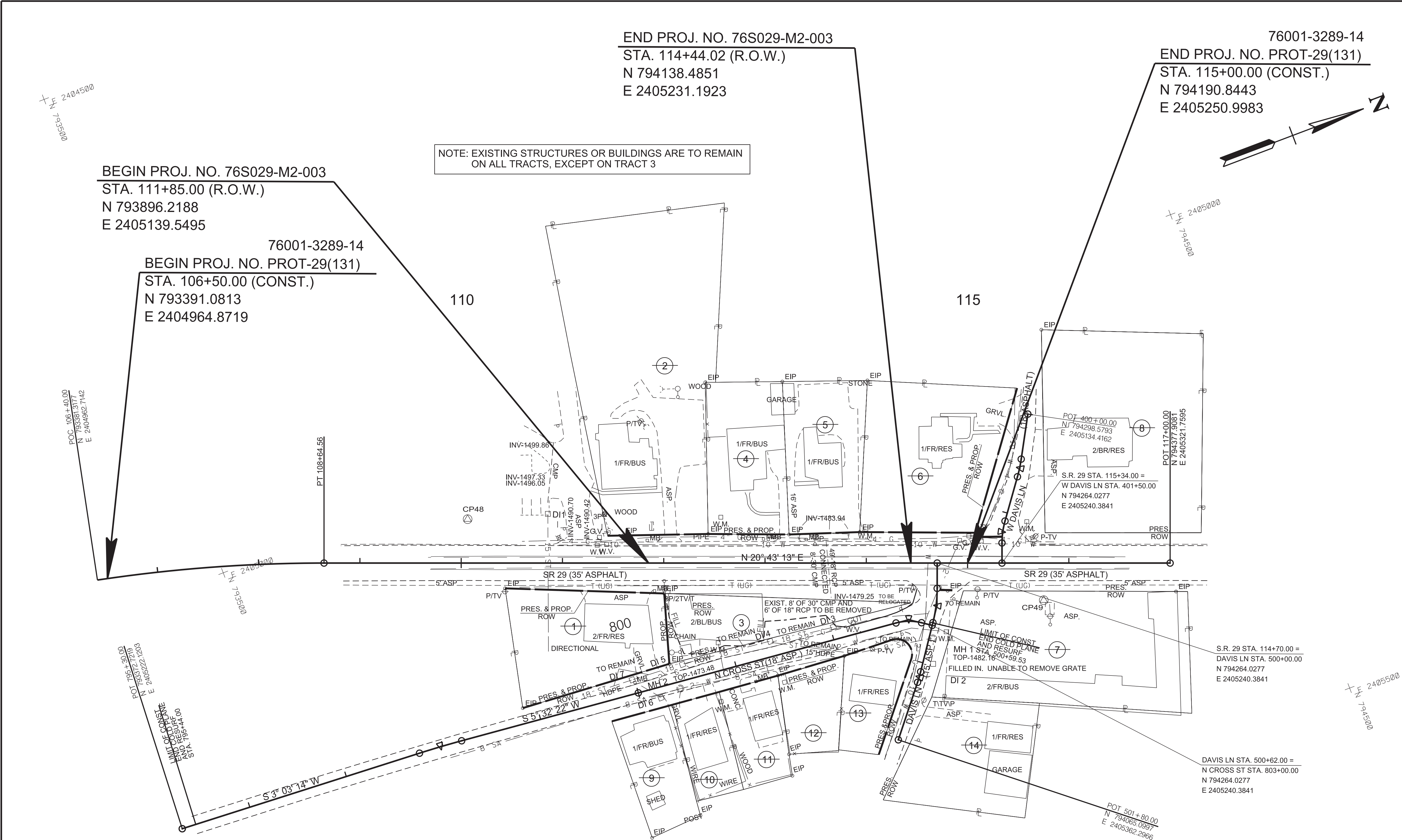
END PROJ. NO. 76S029-M2-003
STA. 114+44.02 (R.O.W.)
N 794138.4851
E 2405231.1923

76001-3289-14
END PROJ. NO. PROT-29(131)
STA. 115+00.00 (CONST.)
N 794190.8443
E 2405250.9983

BEGIN PROJ. NO. 76S029-M2-003
STA. 111+85.00 (R.O.W.)
N 793896.2188
E 2405139.5495

76001-3289-14
BEGIN PROJ. NO. PROT-29(131)
STA. 106+50.00 (CONST.)
N 793391.0813
E 2404964.8719

NOTE: EXISTING STRUCTURES OR BUILDINGS ARE TO REMAIN
ON ALL TRACTS, EXCEPT ON TRACT 3



PROPERTY OWNER LIST

#	OWNER NAME
1	PATRICIA R. MINOR
2	EUGENE OLMSTEAD AND WIFE, MAE OLMSTEAD
3	MARY ANN DELK FROMAN, JIMMY BRUCE DELK, PHYLLIS D. DELK JOHNSON, ERIC WAYNE DELK, AND JOSHUA CORY SIBLEY (WITH LIFE ESTATE JESSE DELK, JESSIE NELL DELK SIBLEY AND HUSBAND, JOSEPH C.SIBLEY)
4	ANNA LOU STEPHENS

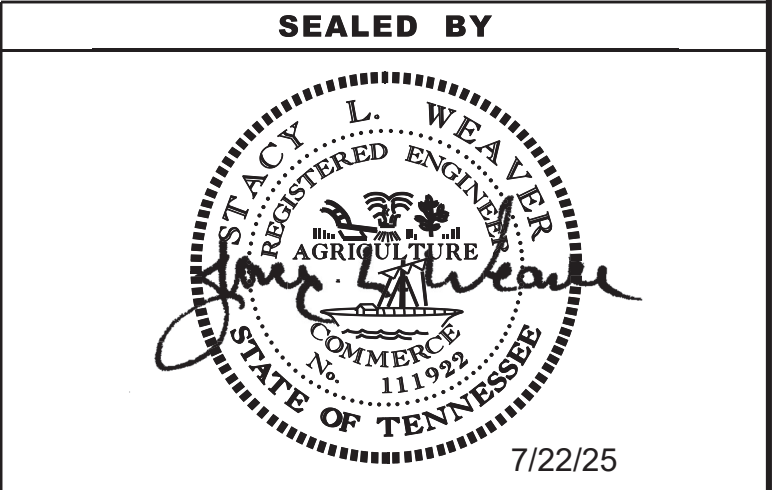
CONTROL POINTS					
POINT	NORTH	EAST	ELEV.	STATION	OFFSET
CP48	793744.4660	2405035.8030	1497.01	110+06.36	43.34' (LT)
CP49	794248.2040	2405311.9130	1488.09	115+75.20	36.68' (RT)

CONTROL POINTS IN REFERENCE TO SR29

PROPERTY OWNER LIST

5	TIMOTHY WAYNE WEST
6	BERNARD TERRY
7	TRACOB, LLC
8	FELICIA GATES, ET AL
9	DARRELL DUNCAN, AND WIFE, CONNIE DUNCAN
10	LESSIE CHAMBERS AND DAUGHTER, MARTHA LOU CHAMBERS
11	MELISSA ANN CHAMBERS
12	JAMES SCOT BOWLING
13	J.J. DOWER AND WIFE, TRACI A. DOWER
14	ROBERT WRIGHT AND AMY WRIGHT

W DAVIS LN PI 400+53.89 N 794,272.3710 E 2,405,181.5013 Δ 7° 17' 28" (RT) D 40° 55' 32" R 140.00 L 17.82 T 8.92 PC 400+44.97 PT 400+62.79 BK BEARING S60°53'56"E AH BEARING S53°36'28"E	W DAVIS LN PI 401+19.26 N 794,233.5717 E 2,405,234.1424 Δ 15° 40' 19" (LT) D 71° 37' 11" R 80.00 L 21.88 T 11.01 PC 401+08.25 PT 401+30.13 BK BEARING S53°36'28"E AH BEARING S69°16'47"E	DAVIS LN PI 500+42.17 N 794,147.8200 E 2,405,279.8168 Δ 13° 58' 11" (RT) D 40° 55' 32" R 140.00 L 34.13 T 17.15 PC 500+25.02 PT 500+59.15 BK BEARING S69°13'06"E AH BEARING S55°14'55"E	DAVIS LN PI 501+14.68 N 794,106.3965 E 2,405,339.5256 Δ 4° 27' 17" (RT) D 40° 55' 32" R 140.00 L 10.89 T 5.45 PC 501+09.23 PT 501+20.12 BK BEARING S55°14'55"E AH BEARING S50°47'38"E	N CROSS ST PI 802+76.67 N 794,116.7895 E 2,405,282.6258 Δ 29° 12' 43" (RT) D 114° 35' 30" R 50.00 L 25.49 T 13.03 PC 802+63.64 PT 802+89.13 BK BEARING N34°45'05"E AH BEARING N5°32' 22"E
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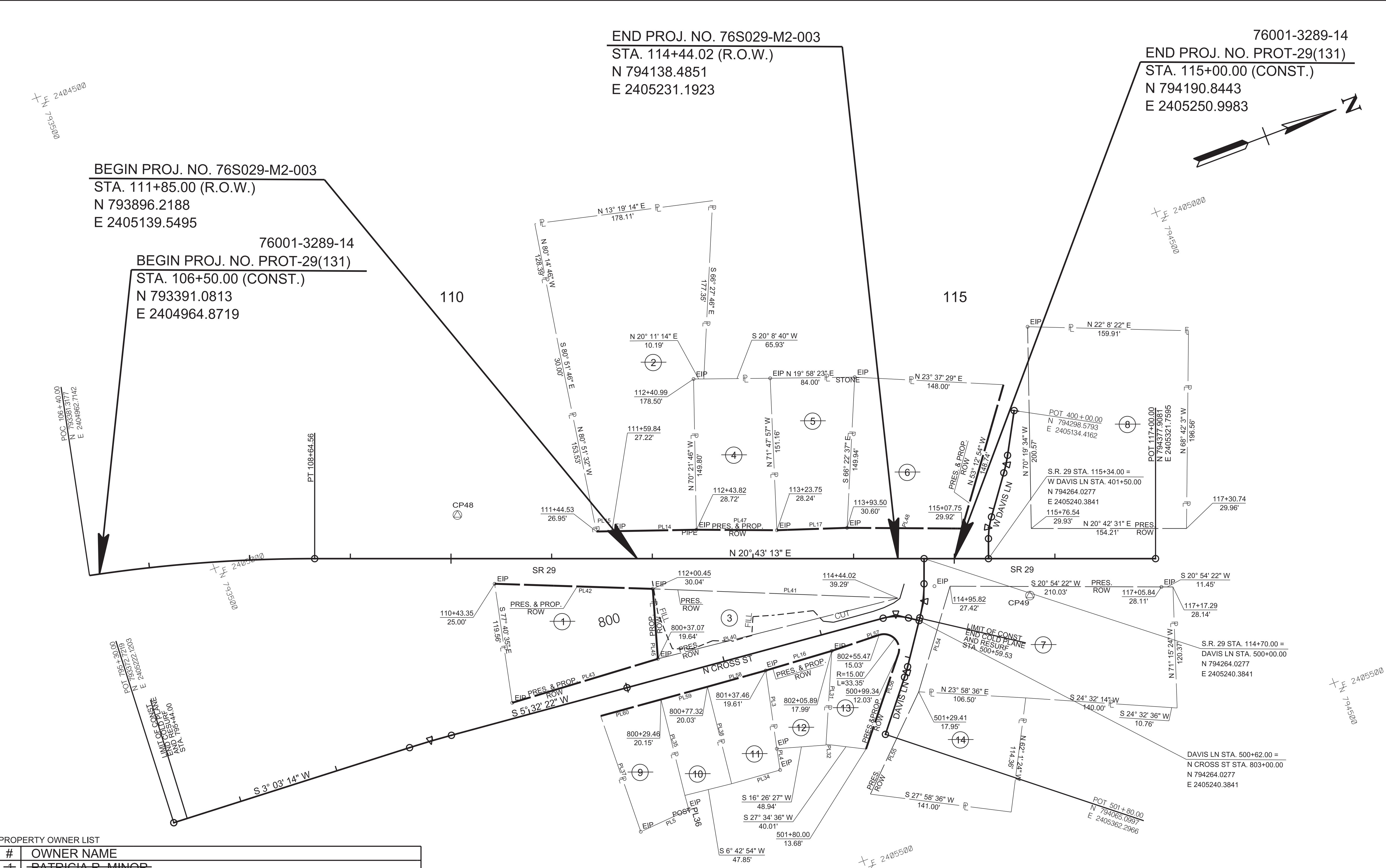
COORDINATES ARE NAD 83(2011), ARE DATUM ADJUSTED BY THE FACTOR OF 1.00005 AND TIED TO THE TGRN. ALL ELEVATIONS ARE REFERENCED TO THE NAVD 1988 WITH GEOID 18.

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PRESENT
LAYOUT

STA. 106+50.00 TO STA. 115+00.00
SCALE: 1"=50'

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2022	76S029-M2-003	4A
PS&E	2025	PROT-29(131)	4A



#	OWNER NAME
1	PATRICIA R. MINOR
2	EUGENE OLMSTEAD AND WIFE, MAE OLMSTEAD
3	MARY ANN DELK FROMAN, JIMMY BRUCE DELK, PHYLLIS D. DELK JOHNSON, ERIC WAYNE DELK, AND JOSHUA CORY SIBLEY (WITH LIFE ESTATE JESSE DELK, JESSIE NELL DELK SIBLEY AND HUSBAND, JOSEPH C.SIBLEY)
4	ANNA LOU STEPHENS

CONTROL POINTS					
POINT	NORTH	EAST	ELEV.	STATION	OFFSET
CP48	793744.4660	2405035.8030	1497.01	110+06.36	43.34' (LT)
CP49	794248.2040	2405311.9130	1488.09	115+75.20	36.68' (RT)

CONTROL POINTS IN REFERENCE TO SR29

PROPERTY OWNER LIST	
5	TIMOTHY WAYNE WEST
6	BERNARD TERRY
7	TRACOBI, LLC
8	FELICIA GATES, ET AL
9	DARRELL DUNCAN, AND WIFE, CONNIE DUNCAN
10	LESSIE CHAMBERS AND DAUGHTER, MARTHA LOU CHAMBERS
11	MELISSA ANN CHAMBERS
12	JAMES SCOT BOWLING
13	J.J. DOWER AND WIFE, TRACIA DOWER
14	ROBERT WRIGHT AND AMY WRIGHT

PROPERTY LINE BEARING AND DISTANCES		
LINE #	BEARING	DISTANCE
PL3	S 77° 20' 48.95" E	78.52'
PL4	S 78° 05' 57.64" E	21.40'
PL5	S 04° 00' 10.01" E	52.47'
PL11	S 80° 51' 32.18" E	153.53'
PL31	S 65° 44' 06.91" E	92.99'
PL32	S 65° 44' 06.91" E	00.76'
PL34	S 04° 52' 59.49" W	50.09'
PL35	S 83° 17' 5.55" E	98.26'
PL36	S 84° 30' 52.42" E	14.63'
PL37	N 88° 24' 06.31" W	121.70'
PL38	S 83° 17' 5.55" E	99.36'
PL45	N 73° 28' 01.54" W	69.71'

ROW LINE BEARING AND DISTANCES		
LINE #	BEARING	DISTANCE
PL14	S 19° 41' 54.78" W	83.99'
PL15	S 19° 41' 54.78" W	15.31'
PL16	S 04° 11' 09.03" W	68.45'
PL17	S 18° 46' 37.62" W	69.79'
PL40	N 06° 32' 01.15" E	245.98'
PL41	S 22° 53' 35.71" W	243.74'
PL42	S 22° 33' 36.60" W	157.18'
PL43	N 3° 55' 08.47" E	151.19'
PL47	N 21° 04' 00.76" E	79.93'
PL48	N 21° 03' 49.19" E	114.24'
PL54	S 52° 45' 23.88" E	109.45'
PL55	S 44° 00' 56.90" E	112.44'
PL56	N 50° 30' 28.27" W	99.70'
PL57	S 02° 07' 8.01" W	49.67'
PL58	S 05° 08' 24.98" W	60.14'
PL59	S 05° 23' 50.63" W	47.87'
PL60	S 05° 23' 50.63" W	62.11'

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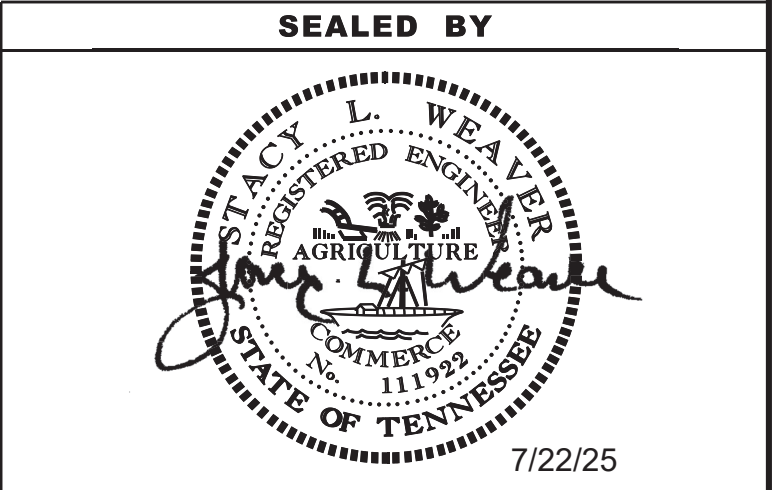
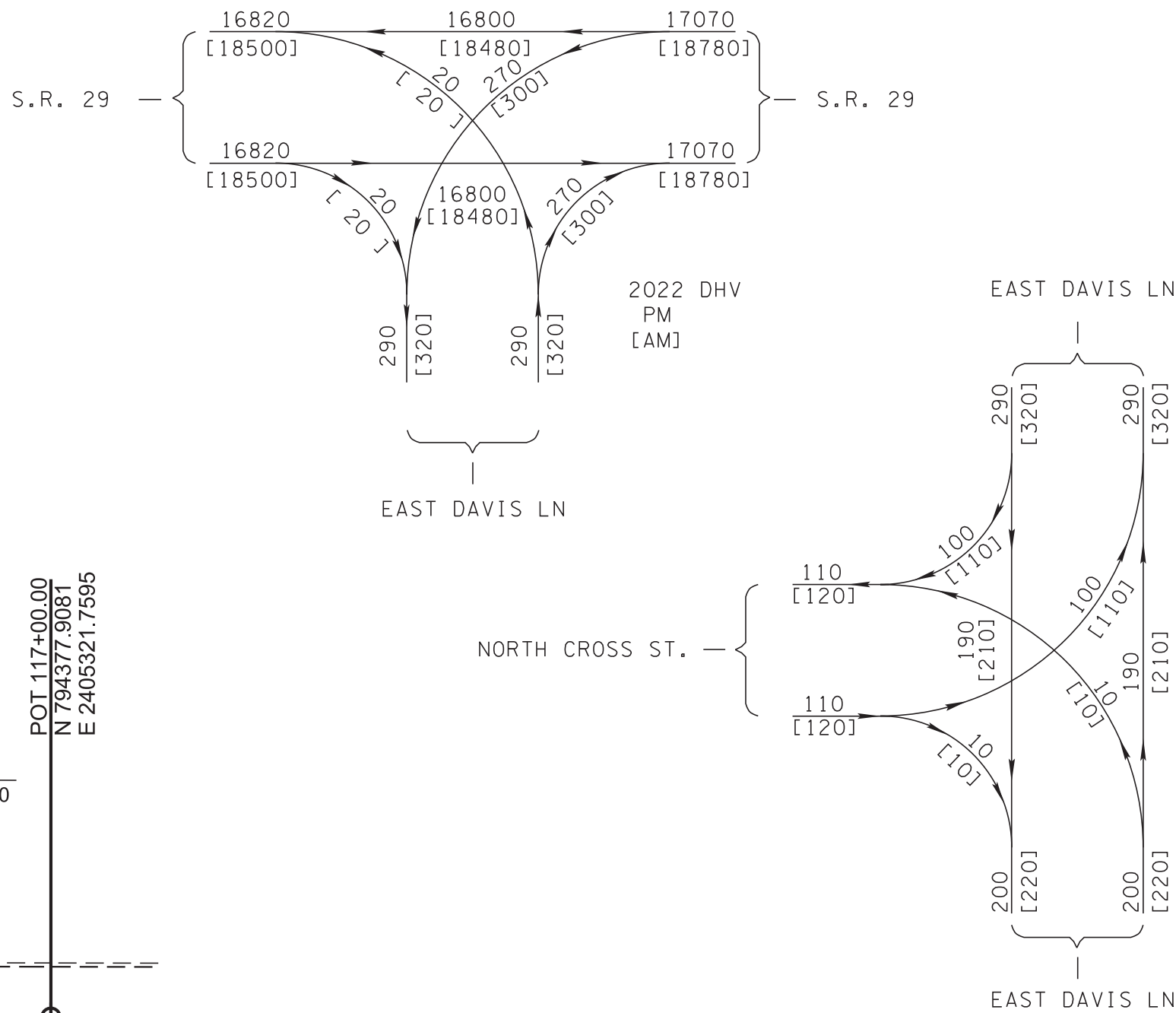
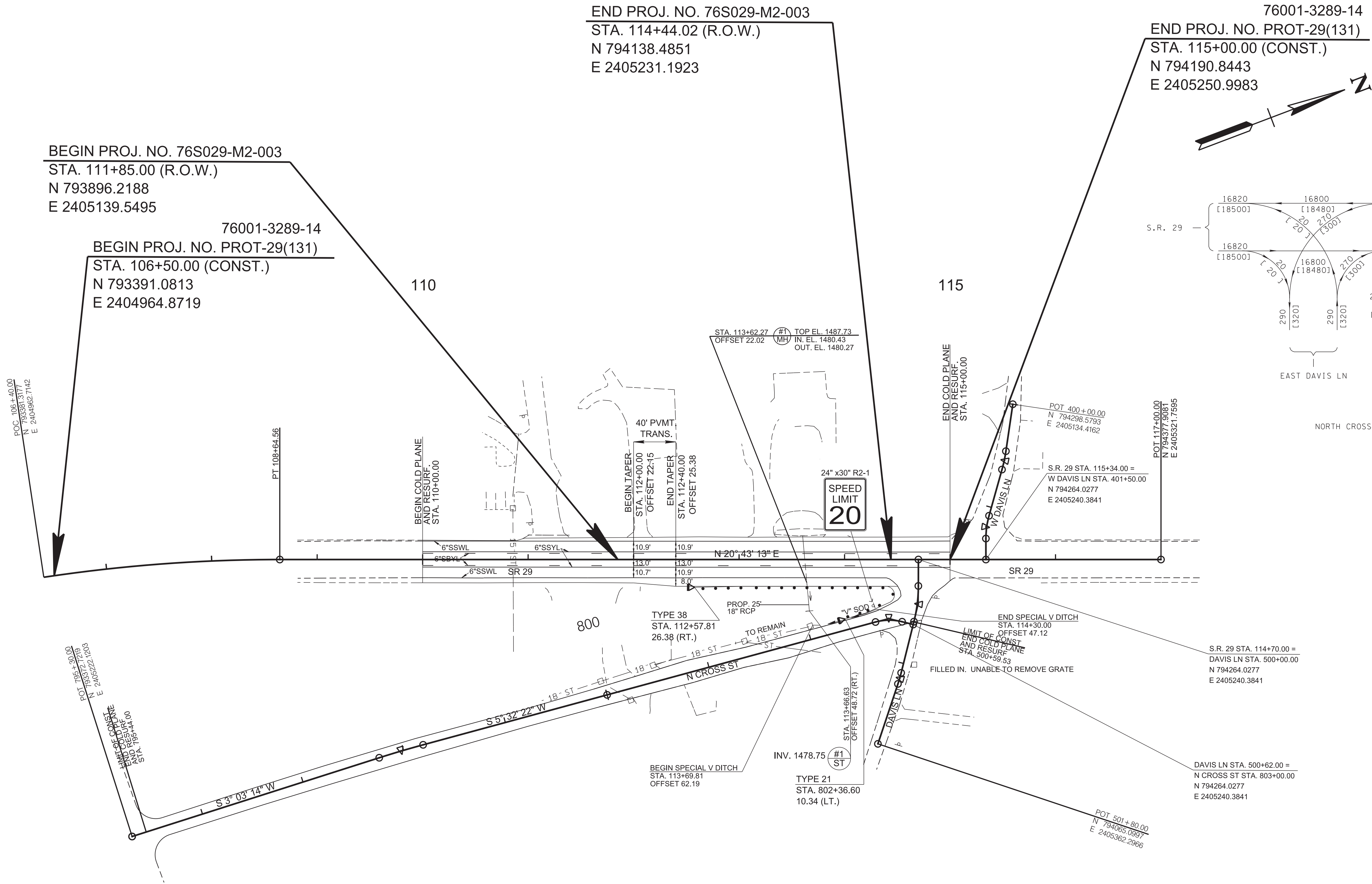
COORDINATES ARE NAD 83(2011), ARE DATUM ADJUSTED BY THE FACTOR OF 1.00005 AND TIED TO THE TGRN. ALL ELEVATIONS ARE REFERENCED TO THE NAVD 1988 WITH GEOID 18.

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

RIGHT-OF-WAY
DETAILS

STA.106+50.00 TO STA.115+00.00
SCALE: 1"=50'

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2022	76S029-M2-003	4B
PS&E	2025	PROT-29(131)	4B



COORDINATES ARE NAD 83(2011), ARE DATUM ADJUSTED BY THE FACTOR OF 1.00005 AND TIED TO THE TGRN. ALL ELEVATIONS ARE REFERENCED TO THE NAVD 1988 WITH GEOID 18.

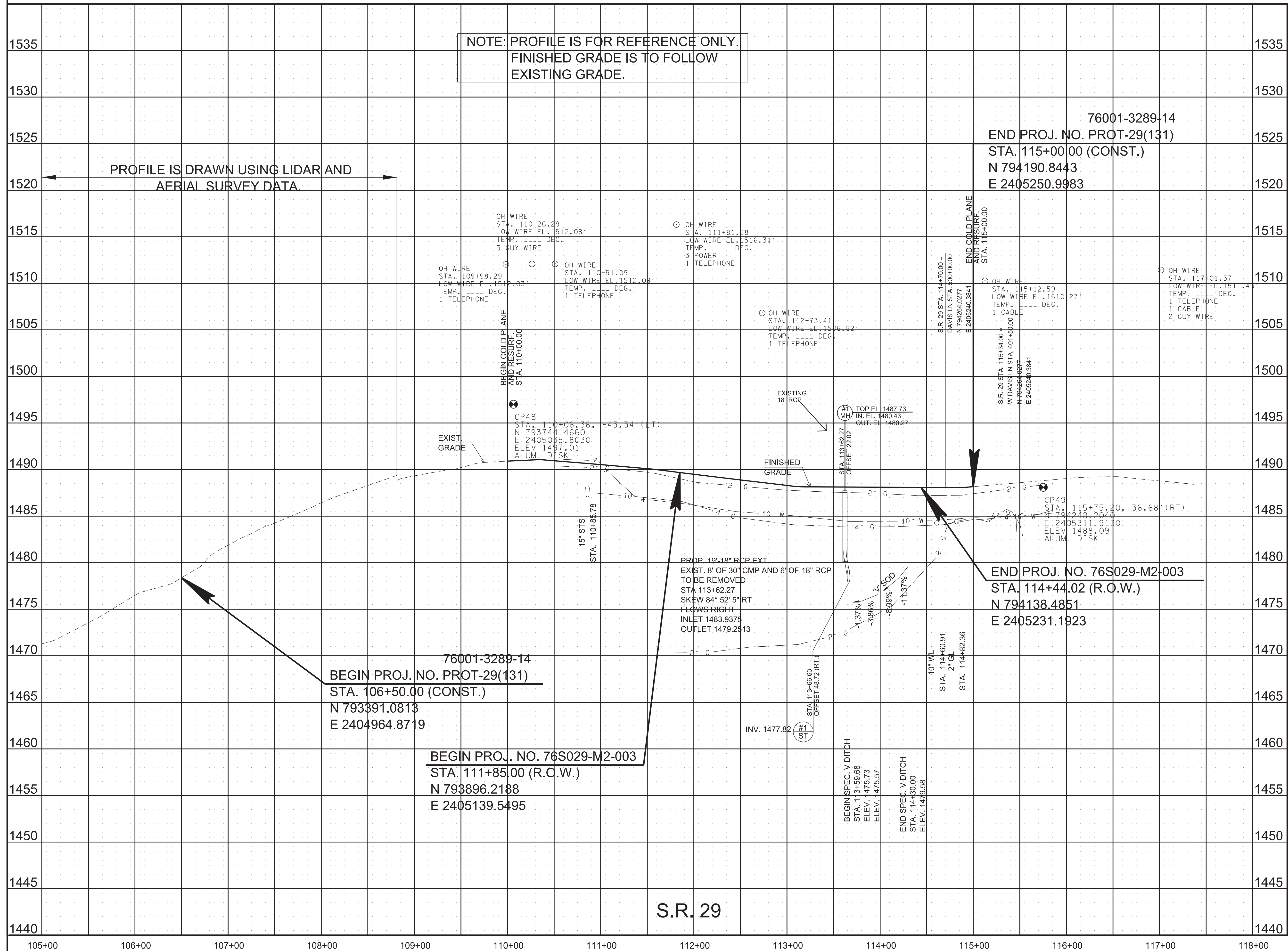
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PROPOSED
LAYOUT

STA.106+50.00 TO STA.115+00.00
SCALE: 1"=50'

SEE SHEET 4C FOR S.R. 29 PROFILE
SEE SHEET 5 FOR N CROSS ST PROFILE

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2022	76S029-M2-003	4C
PS&E	2025	PROT-29(131)	4C



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SEAL BY

STACY L. WEAVER
REGISTERED ENGINEER
IN THE FIELD OF
AGRICULTURE
COMMENCE
NO. 111923
STATE OF TENNESSEE

7/22/25

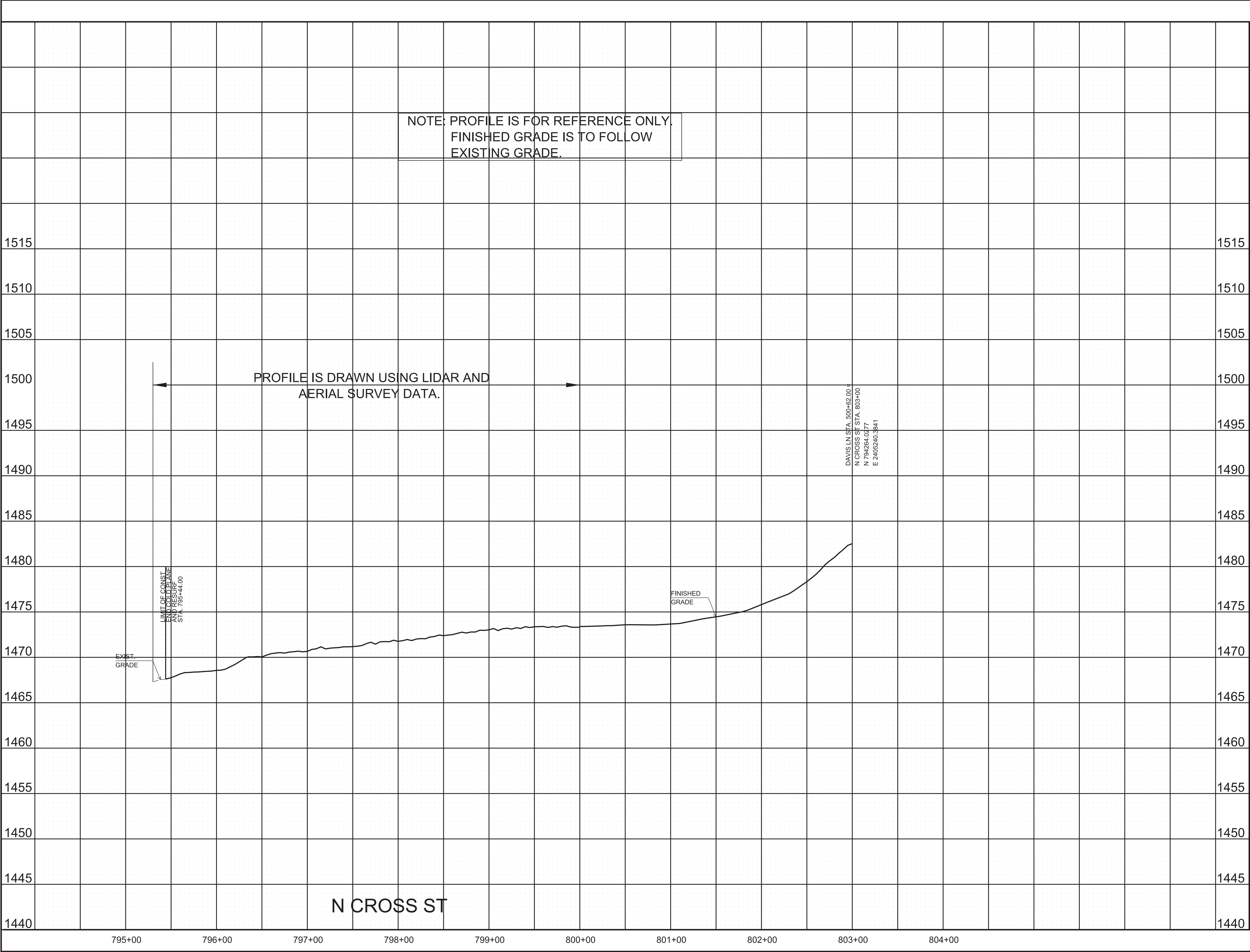
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PROPOSED PROFILE

STA. 110+00.00 TO STA. 115+00.00


SCALE: 1"= 50' HORIZ.
1"= 5' VERT.

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TYPE	YEAR	PROJECT NO.	SHEET NO.
PS&E	2025	PROT-29(131)	5

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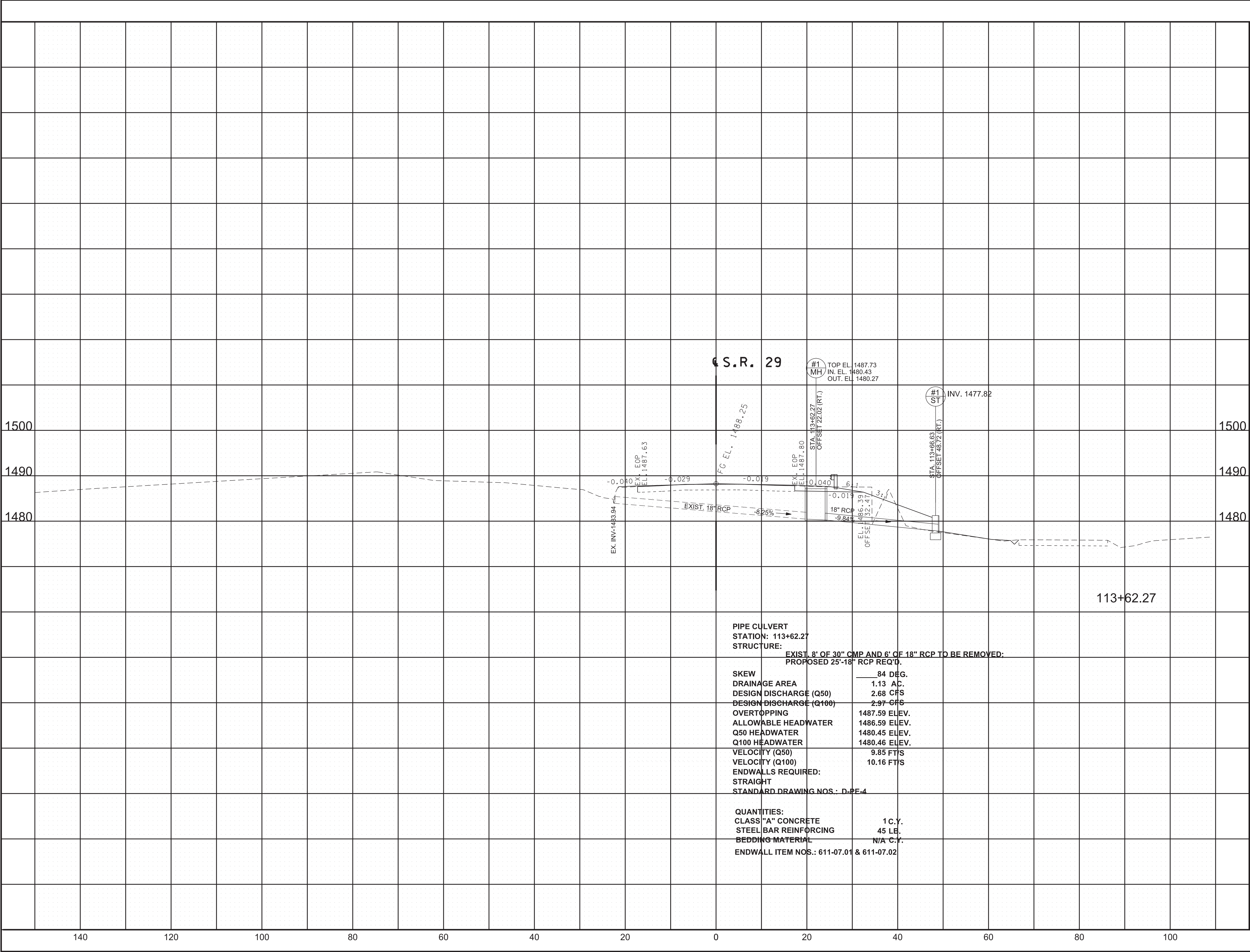


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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

**SIDE ROAD
PROFILE**
STA. 795+00.00 TO STA. 804+00.00
SCALE: 1"= 50' HORIZ.
1"= 5' VERT.

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TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2022	76S029-M2-003	6
PS&E	2025	PROT-29(131)	7

SEAL BY

7/22/25

**STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION**

**CULVERT
SECTION**

SCALE: 1"=10' HORIZ.
1"=10' VERT.

EROSION PREVENTION AND SEDIMENT CONTROL GENERAL NOTES

DISTURBED AREA

- (1) IF DISTURBED ACREAGE IS EQUAL TO ONE ACRE OR MORE, PLEASE CONTACT TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION AS SOON AS POSSIBLE BECAUSE AN NPDES PERMIT WILL BE REQUIRED.
- (2) AREAS TO BE UNDISTURBED SHALL BE CLEARLY MARKED IN THE FIELD BEFORE CONSTRUCTION ACTIVITIES BEGIN.
- (3) UNLESS OTHERWISE NOTED IN THE PLANS, THE CONTRACTOR SHALL NOT CLEAR/DISTURB ANY AREA BEYOND 15 FEET FROM SLOPE LINES.
- (4) PRE-CONSTRUCTION VEGETATIVE GROUND COVER SHALL NOT BE DESTROYED, REMOVED OR DISTURBED (I.E. CLEARING AND GRUBBING INITIATED) MORE THAN 14 CALENDAR DAYS PRIOR TO GRADING OR EARTH MOVING ACTIVITIES UNLESS THE AREA IS MULCHED, SEEDED WITH MULCH, OR OTHER TEMPORARY COVER IS APPLIED.
- (5) CLEARING, GRUBBING, AND OTHER DISTURBANCE TO RIPARIAN VEGETATION SHALL BE LIMITED TO THE MINIMUM NECESSARY FOR SLOPE CONSTRUCTION AND EQUIPMENT OPERATIONS. EXISTING VEGETATION, INCLUDING STREAM AND WETLAND BUFFERS (UNLESS PERMITTED), SHOULD BE PRESERVED TO THE MAXIMUM EXTENT POSSIBLE. UNNECESSARY VEGETATION REMOVAL IS PROHIBITED.

SEDIMENT CONTROL

- (6) EPSC MEASURES SHALL BE INSTALLED AND FUNCTIONAL PRIOR TO ANY EARTH MOVING OPERATIONS AND SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD EXCEPT AS SUCH WORK MAY BE NECESSARY TO INSTALL EPSC MEASURES.
- (7) TEMPORARY EPSC MEASURES MAY BE REMOVED AT THE BEGINNING OF THE WORKDAY BUT MUST BE REINSTALLED AT THE END OF THE WORKDAY OR BEFORE/DURING A PRECIPITATION EVENT.
- (8) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT THE OFFSITE MIGRATION OR DEPOSIT OF SEDIMENT OFF THE PROJECT LIMITS (E.G. R.O.W., EASEMENTS, ETC.), INTO WATERS OF THE STATE/U.S., OR ONTO ROADWAYS USED BY THE GENERAL PUBLIC. IF SEDIMENT ESCAPES THE CONSTRUCTION SITE, OFFSITE ACCUMULATIONS OF SEDIMENT THAT HAVE NOT REACHED A STREAM MUST BE REMOVED AT A FREQUENCY SUFFICIENT TO MINIMIZE OFFSITE IMPACTS (E.G., FUGITIVE SEDIMENT THAT HAS ESCAPED THE CONSTRUCTION SITE AND HAS COLLECTED IN A STREET MUST BE REMOVED SO THAT IT IS NOT SUBSEQUENTLY WASHED INTO STORM SEWERS AND STREAMS BY THE NEXT RAIN AND/OR SO THAT IT DOES NOT POSE A SAFETY HAZARD TO USERS OF PUBLIC STREETS). ARRANGEMENTS CONCERNING REMOVAL OF SEDIMENT ON ADJOINING PROPERTY MUST BE NEGOTIATED WITH THE ADJOINING PROPERTY OWNER BEFORE REMOVAL OF SEDIMENT.
- (9) OFFSITE VEHICLE TRACKING OF SEDIMENTS AND THE GENERATION OF DUST SHALL BE MINIMIZED. A STABILIZED CONSTRUCTION EXIT (A POINT OF ENTRANCE/EXIT TO THE CONSTRUCTION PROJECT) SHALL BE PROVIDED TO REDUCE THE TRACKING OF MUD AND DIRT ONTO PUBLIC ROADS BY CONSTRUCTION VEHICLES.
- (10) THE DEWATERING OF WORK AREAS, TRENCHES, FOUNDATIONS, EXCAVATIONS, ETC. THAT HAVE COLLECTED STORMWATER, WATER FROM VEHICLE WASH AREAS, OR GROUNDWATER SHALL BE EITHER HELD IN SETTLING BASINS OR TREATED BY FILTRATION AND/OR CHEMICAL TREATMENT PRIOR TO ITS DISCHARGE. ALL PHYSICAL AND/OR CHEMICAL TREATMENT WILL BE APPLIED IN ACCORDANCE WITH THE MANUFACTURER'S GUIDELINES AND FULLY DESCRIBED IN THE EPSC PLANS. WATER DISCHARGED SHALL NOT CAUSE AN OBJECTIONABLE COLOR CONTRAST WITHIN THE RECEIVING NATURAL RESOURCE. WATER MUST BE HELD IN SETTLING BASINS UNTIL AT LEAST AS CLEAR AS THE RECEIVING WATERS. SETTLING BASINS SHALL NOT BE LOCATED CLOSER THAN 20 FEET FROM THE TOP BANK OF A STREAM. SETTLING BASINS AND SEDIMENT TRAPS SHALL BE PROPERLY DESIGNED ACCORDING TO THE SIZE OF THE DRAINAGE AREAS OR VOLUME OF WATER TO BE TREATED. TREATED WATER MUST BE DISCHARGED THROUGH A PIPE OR WELL-VEGETATED OR LINED CHANNEL, SO THAT THE DISCHARGE DOES NOT CAUSE EROSION OR SEDIMENT TRANSPORT. DISCHARGES FROM BASINS AND IMPOUNDMENTS SHALL UTILIZE OUTLET STRUCTURES THAT ONLY WITHDRAW WATER FROM NEAR THE SURFACE OF THE BASIN OR IMPOUNDMENT. DISCHARGES MUST NOT CAUSE AN OBJECTIONABLE COLOR CONTRAST WITH THE RECEIVING STREAM.

INSPECTION, MAINTENANCE & REPAIR

- (11) THE TDOT CONSTRUCTION SUPERVISOR (OR THEIR DESIGNEE) AND THE CONTRACTOR'S RESPONSIBLE PARTY ARE RESPONSIBLE FOR INSPECTIONS. MAINTENANCE AND REPAIR ACTIVITIES ARE THE RESPONSIBILITY OF THE CONTRACTOR. THE TDOT CONSTRUCTION SUPERVISOR OR THEIR DESIGNEE SHALL COMPLETE THE EPSC INSPECTION REPORTS AND DISTRIBUTE COPIES PER THE CONTRACT.
- (12) TDOT CONSULTANTS AND CONTRACTOR STAFF RESPONSIBLE FOR THE INSPECTION, IMPLEMENTATION, MAINTENANCE, AND/OR REPAIR OF EPSC MEASURES SHALL SUCCESSFULLY COMPLETE THE TDEC "LEVEL 1 - FUNDAMENTALS OF EROSION PREVENTION AND SEDIMENT CONTROL FOR CONSTRUCTION SITES" COURSE AND ANY REFRESHER COURSES AS REQUIRED TO MAINTAIN CERTIFICATION. TDOT STAFF AND SUPERVISORS RESPONSIBLE FOR THE INSPECTION, IMPLEMENTATION, MAINTENANCE, AND/OR REPAIR OF EPSC MEASURES SHALL SUCCESSFULLY COMPLETE THE TDOT "FUNDAMENTALS OF EROSION AND SEDIMENT CONTROL" CLASS AND ANY REFRESHER COURSES AS REQUIRED TO MAINTAIN CERTIFICATION.
- (13) EPSC CONTROLS SHALL BE INSPECTED ACCORDING TO PERMIT REQUIREMENTS TO VERIFY MEASURES HAVE BEEN INSTALLED AND MAINTAINED IN ACCORDANCE WITH TDOT STANDARD DRAWINGS, SPECIFICATIONS, AND GOOD ENGINEERING PRACTICES. EPSC INSPECTIONS SHALL BE DOCUMENTED ON THE TDOT EPSC INSPECTION REPORT.
- (14) DISCHARGE POINTS SHALL BE INSPECTED TO ASCERTAIN WHETHER EPSC MEASURES ARE EFFECTIVE IN PREVENTING EROSION AND CONTROLLING SEDIMENT INCLUDING SIGNIFICANT IMPACTS TO SURROUNDING NATURAL RESOURCES AND ADJACENT PROPERTY OWNERS. WHERE DISCHARGE LOCATIONS ARE INACCESSIBLE, NEARBY DOWN GRADIENT LOCATIONS SHALL BE INSPECTED. LOCATIONS WHERE VEHICLES ENTER AND EXIT THE SITE SHALL BE INSPECTED FOR EVIDENCE OF OFFSITE ROADWAY SEDIMENT TRACKING.
- (15) UPON CONCLUSION OF THE INSPECTIONS, EPSC MEASURES FOUND TO BE INEFFECTIVE SHALL BE REPAIRED, REPLACED, OR MODIFIED BEFORE THE NEXT RAIN EVENT, IF POSSIBLE, BUT IN NO CASE MORE THAN 24 HOURS AFTER THE INSPECTION OR WHEN THE CONDITION IS IDENTIFIED. IF THE REPAIR, REPLACEMENT OR MODIFICATION IS NOT PRACTICAL WITHIN THE 24 HOUR TIMEFRAME, WRITTEN DOCUMENTATION SHALL BE PROVIDED IN THE FIELD DIARY AND EPSC INSPECTION REPORT. AN ESTIMATED REPAIR, REPLACEMENT OR MODIFICATION SCHEDULE SHALL BE DOCUMENTED WITHIN 24 HOURS AFTER IDENTIFICATION.
- (16) INSPECTION, REPAIR, AND MAINTENANCE OF EPSC MEASURES SHALL BE PERFORMED ON A REGULAR BASIS. SEDIMENT SHALL BE REMOVED FROM SEDIMENT CONTROL STRUCTURES WHEN THE DESIGN CAPACITY HAS BEEN REDUCED BY FIFTY PERCENT (50%). DURING SEDIMENT REMOVAL, THE CONTRACTOR SHALL TAKE STEPS TO ENSURE THAT STRUCTURAL COMPONENTS OF EPSC MEASURES ARE NOT DAMAGED AND THUS MADE INEFFECTIVE. IF DAMAGE DOES OCCUR, THE CONTRACTOR SHALL REPAIR THE EPSC MEASURES AT THE CONTRACTOR'S OWN EXPENSE.
- (17) THE EPSC PLAN SHALL BE UPDATED WHENEVER EPSC INSPECTIONS INDICATE, OR WHERE STATE OR FEDERAL OFFICIALS DETERMINE EPSC MEASURES ARE PROVING INEFFECTIVE IN ELIMINATING OR SIGNIFICANTLY MINIMIZING POLLUTANT SOURCES OR ARE OTHERWISE NOT ACHIEVING THE GENERAL OBJECTIVES OF CONTROLLING POLLUTANTS IN STORM WATER DISCHARGES ASSOCIATED WITH THE CONSTRUCTION ACTIVITY.
- (18) SEDIMENT REMOVED FROM SEDIMENT CONTROL STRUCTURES SHALL BE PLACED AND TREATED IN A MANNER SO THAT THE SEDIMENT IS CONTAINED WITHIN THE PROJECT LIMITS AND DOES NOT MIGRATE ONTO ADJACENT PROPERTIES AND INTO WATERS OF THE STATE/U.S. COST FOR THIS TREATMENT SHALL BE INCLUDED IN PRICE BID FOR ITEM NO. 209-05 SEDIMENT REMOVAL, C.Y.

EROSION PREVENTION

- (19) CONSTRUCTION SHALL BE SEQUENCED AND STAGED TO MINIMIZE THE EXPOSURE TIME OF GRADED OR DENUDED SOIL AREAS, PRESERVE TOPSOIL, AND MINIMIZE SOIL COMPACTION.
- (20) THE ACCEPTED EPSC PLAN SHALL REQUIRE THAT EPSC MEASURES BE IN PLACE BEFORE CLEARING, GRUBBING, EXCAVATION, GRADING, CULVERT OR BRIDGE CONSTRUCTION, CUTTING, FILLING, OR ANY OTHER EARTHWORK OCCURS, EXCEPT AS SUCH WORK MAY BE NECESSARY TO INSTALL EPSC MEASURES.
- (21) NO WORK SHALL BE STARTED UNTIL THE CONTRACTOR'S PLAN FOR THE STAGING OF OPERATIONS, INCLUDING THE PLAN FOR STAGING OF TEMPORARY AND PERMANENT EPSC MEASURES, HAS BEEN ACCEPTED BY THE TDOT RESPONSIBLE PARTY. THE CONTRACTOR'S EPSC PLAN SHALL INCORPORATE AND SUPPLEMENT, AS ACCEPTABLE, THE BASIC EPSC DEVICES ON THE EPSC PLAN.

- (22) TEMPORARY STABILIZATION SHALL BE INITIATED WITHIN 14 CALENDAR DAYS WHEN CONSTRUCTION ACTIVITIES ON A PORTION OF THE SITE ARE TEMPORARILY CEASED AND EARTH DISTURBING ACTIVITIES WILL NOT RESUME UNTIL AFTER 14 CALENDAR DAYS. PERMANENT STABILIZATION MEASURES IN DISTURBED AREAS SHALL BE INITIATED WITHIN 14 CALENDAR DAYS AFTER FINAL GRADING OF ANY PHASE OF CONSTRUCTION.
- (23) STEEP SLOPES SHALL BE TEMPORARILY STABILIZED NOT LATER THAN 7 DAYS AFTER CONSTRUCTION ACTIVITY ON THE SLOPE HAS TEMPORARILY OR PERMANENTLY CEASED. STEEP SLOPES ARE DEFINED AS A NATURAL OR CREATED SLOPE OF 35% GRADE OR GREATER REGARDLESS OF HEIGHT.
- (24) PERMANENT STABILIZATION WILL REPLACE TEMPORARY MEASURES AS SOON AS PRACTICABLE. PRIORITY SHALL BE GIVEN TO FINISHING OPERATIONS AND PERMANENT EPSC MEASURES OVER TEMPORARY EPSC MEASURES ON ALL PROJECTS.
- (25) TEMPORARY OR PERMANENT STABILIZATION MUST BE FREE OF FINES (SILT AND CLAY SIZED PARTICLES). UNPACKED GRAVEL CONTAINING FINES OR CRUSHER-RUN WILL NOT BE CONSIDERED SUFFICIENT STABILIZATION.
- (26) DELAYING THE PLANTING OF COVER VEGETATION UNTIL WINTER MONTHS OR DRY MONTHS SHOULD BE AVOIDED.

PERMITS, PLANS & RECORDS

- (27) THE EPSC PLAN IS TO SERVE AS AN INITIAL GUIDE FOR SITE PERSONNEL AS THE CONSTRUCTION PROCESS DEVELOPS. IT MUST BE AMENDED, MODIFIED, AND UPDATED WHENEVER A CHANGE IN THE DESIGN OR CONSTRUCTION OF THE PROJECT OCCURS. THE STAGES DEPICTED IN THE EPSC PLANS MAY NOT COINCIDE WITH THE ACTUAL PHASES OF CONSTRUCTION ESTABLISHED BY THE CONTRACTOR DURING CONSTRUCTION, THUS MODIFICATIONS WILL BE REQUIRED TO ENSURE THE EPSC PLAN IS MAINTAINED TO DEPICT CURRENT SITE CONDITIONS. IT SHOULD BE MAINTAINED SUCH THAT IT WILL ALWAYS REFLECT THE MEASURES THAT ARE INSTALLED DURING THE VARIOUS PHASES OF CONSTRUCTION. IT IS IMPRACTICAL TO DETERMINE ALL THE INTERMEDIATE PHASES OF CONSTRUCTION THAT WILL OCCUR; THUS THESE DOCUMENTS WILL HAVE TO BE UPDATED THROUGHOUT THE LIFE OF THE CONSTRUCTION PROJECT.

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2022	76S029-M2-003	7
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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

EROSION
PREVENTION &
SEDIMENT CONTROL
(EPSC) NOTES

GOOD HOUSEKEEPING MEASURES & WASTE DISPOSAL

- (29)

THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT LITTER AND CONSTRUCTION WASTES FROM ENTERING WATERS OF THE STATE/U.S. THESE MATERIALS SHALL BE REMOVED FROM STORMWATER EXPOSURE PRIOR TO ANTICIPATED STORM EVENTS OR BEFORE BEING CARRIED OFFSITE BY WIND, OR OTHERWISE PREVENTED FROM BECOMING A POLLUTANT SOURCE FOR STORMWATER DISCHARGES. AFTER USE, MATERIALS USED FOR EPSC SHALL BE REMOVED FROM THE SITE.
- (30)

THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS TO ENSURE THAT PETROLEUM PRODUCTS OR OTHER CHEMICAL POLLUTANTS ARE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. ALL EQUIPMENT REFUELING, SERVICING, AND STAGING AREAS SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL LAWS, RULES, REGULATIONS, AND ORDINANCES, INCLUDING THOSE OF THE NATIONAL FIRE PROTECTION ASSOCIATION. APPROPRIATE CONTAINMENT MEASURES FOR THESE AREAS SHALL BE USED.
- (31)

CONTRACTORS SHALL PROVIDE DESIGNATED TRUCK WASHOUT AREAS ON THE SITE. THESE AREAS MUST BE SELF CONTAINED, NOT CONNECTED TO ANY STORMWATER OUTLET OF THE SITE, AND PROPERLY SIGNED. WASH DOWN OR WASTE DISCHARGE OF CONCRETE TRUCKS SHALL NOT BE PERMITTED ONSITE UNLESS PROPER SETTLEMENT AREAS HAVE BEEN PROVIDED IN ACCORDANCE WITH BOTH STATE AND FEDERAL REGULATIONS.
- (32)

WHEEL WASH WATER SHALL BE COLLECTED AND ALLOWED TO SETTLE OUT SUSPENDED SOLIDS PRIOR TO DISCHARGE. WHEEL WASH WATER SHALL NOT BE DISCHARGED DIRECTLY INTO ANY STORMWATER SYSTEM OR STORMWATER TREATMENT SYSTEM.
- (33)

IF PORTABLE SANITARY FACILITIES ARE PROVIDED ON CONSTRUCTION SITES, SANITARY WASTE SHALL BE COLLECTED FROM THE PORTABLE UNITS IN A TIMELY MANNER BY A LICENSED WASTE MANAGEMENT CONTRACTOR OR AS REQUIRED BY ANY REGULATIONS. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF SANITARY WASTE.
- (34)

ONLY CONSTRUCTION PRODUCTS NEEDED SHALL BE STORED ONSITE BY THE CONTRACTOR. THE CONTRACTOR SHALL STORE ALL MATERIALS UNDER COVER AND IN APPROPRIATE CONTAINERS. PRODUCTS MUST BE STORED IN ORIGINAL CONTAINERS AND LABELED. MATERIAL MIXING SHALL BE CONDUCTED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS. THE CONTRACTOR'S RESPONSIBLE PARTY SHALL INSPECT MATERIALS STORAGE AREAS REGULARLY TO ENSURE PROPER USE AND DISPOSAL.
- (35)

WHEN POSSIBLE, ALL PRODUCTS SHALL BE USED COMPLETELY BEFORE PROPERLY DISPOSING OF THE CONTAINER OFFSITE. THE MANUFACTURER'S DIRECTIONS FOR DISPOSAL OF MATERIALS AND CONTAINERS SHALL BE FOLLOWED.
- (36)

ALL PAINT CONTAINERS SHALL BE TIGHTLY SEALED AND STORED WHEN NOT REQUIRED FOR USE. EXCESS PAINT SHALL BE DISPOSED OF ACCORDING TO THE MANUFACTURER'S INSTRUCTIONS AND APPLICABLE STATE AND LOCAL REGULATIONS.
- (37)

ALL HAZARDOUS WASTE MATERIALS SHALL BE DISPOSED OF IN A MANNER WHICH IS COMPLIANT WITH LOCAL OR STATE REGULATIONS. SITE PERSONNEL SHALL BE INSTRUCTED IN THESE PRACTICES, AND THE INDIVIDUAL DESIGNATED AS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR SEEING THAT THESE PRACTICES ARE FOLLOWED. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF HAZARDOUS MATERIAL.
- (38)

OPEN BURNING IS PROHIBITED UNLESS IT IS SPECIFICALLY ALLOWED BY LAW. IF ALLOWED, NATURAL VEGETATION, TREES, AND UNTREATED LUMBER SHALL BE THE ONLY MATERIALS THAT CAN BE OPEN BURNED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL APPLICABLE STATE AND LOCAL PERMITS PRIOR TO ANY BURNING.
- (39)

DISPOSAL OF ONSITE VEGETATION AND TREES BY CHIPPING THEM INTO MULCH IS PREFERABLE TO OPEN BURNING. THIS MULCH MAY BE USED AS AN ONSITE SOIL STABILIZATION MEASURE WHERE APPROPRIATE.
- (40)

WASTE MATERIAL (EARTH, ROCK, ASPHALT, CONCRETE, ETC.) NOT REQUIRED FOR THE CONSTRUCTION OF THE PROJECT WILL BE DISPOSED OF BY THE CONTRACTOR. IMPACTS TO WATERS OF THE STATE/U.S. SHALL BE AVOIDED IF POSSIBLE. IF UNAVOIDABLE, THE CONTRACTOR WILL OBTAIN ANY AND ALL NECESSARY PERMITS INCLUDING, BUT NOT LIMITED TO NPDES, AQUATIC RESOURCES ALTERATION PERMIT(S), CORPS OF ENGINEERS SECTION 404 PERMITS, AND TVA SECTION 26A PERMITS TO DISPOSE OF WASTE MATERIALS.

SUPPORT ACTIVITIES

- (41)

IF OFFSITE BORROW AND WASTE AREAS BECOME NECESSARY DURING THE LIFE OF THE PROJECT, THIS SUPPORT ACTIVITY SHALL BE ADDRESSED PER THE TDOT WASTE AND BORROW MANUAL.
- (42)

MATERIALS AND STAGING AREAS SHALL BE LOCATED IN NON-WETLAND AREAS AND ABOVE THE 100-YEAR, FEDERAL EMERGENCY MANAGEMENT AGENCY FLOODPLAIN.
- (43)

IT WILL BE THE RESPONSIBILITY OF THE CONTRACTOR TO SUPPLY EPSC PLANS FOR THE MATERIAL AND STAGING AREAS TO THE ENVIRONMENTAL DIVISION COMPLIANCE AND FIELD SERVICES OFFICE FOR REVIEW.

SPILL PREVENTION, MANAGEMENT & NOTIFICATION

- (44)

ALL ONSITE VEHICLES SHALL BE MONITORED FOR LEAKS AND RECEIVE REGULAR PREVENTIVE MAINTENANCE TO REDUCE THE CHANCE OF LEAKAGE AND SPILLS.
- (45)

FOR ALL HAZARDOUS MATERIALS STORED ONSITE, THE MANUFACTURER'S RECOMMENDED METHODS FOR SPILL CLEAN UP SHALL BE CLEARLY POSTED. SITE PERSONNEL SHALL BE MADE AWARE OF THE PROCEDURES AND THE LOCATIONS OF THE INFORMATION AND CLEANUP SUPPLIES.
- (46)

APPROPRIATE CLEANUP MATERIALS AND EQUIPMENT SHALL BE MAINTAINED BY THE CONTRACTOR IN THE MATERIALS STORAGE AREA ONSITE AND UNDER COVER. SPILL RESPONSE EQUIPMENT SHALL BE INSPECTED AND MAINTAINED BY THE CONTRACTOR AS NECESSARY TO REPLACE ANY MATERIALS USED IN SPILL RESPONSE ACTIVITIES.
- (47)

ALL SPILLS SHALL BE CLEANED IMMEDIATELY AFTER DISCOVERY AND THE MATERIALS DISPOSED OF PROPERLY. THE SPILL AREA SHALL BE KEPT WELL VENTILATED AND PERSONNEL WILL WEAR APPROPRIATE PROTECTIVE CLOTHING TO PREVENT INJURY FROM CONTACT WITH A HAZARDOUS SUBSTANCE.
- (48)

THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE THE SPILL PREVENTION AND CLEANUP COORDINATOR. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT THE SITE SUPERINTENDENT HAS HAD APPROPRIATE TRAINING FOR HAZARDOUS MATERIALS HANDLING, SPILL MANAGEMENT, AND CLEANUP.
- (49)

IF AN OIL SHEEN IS OBSERVED ON SURFACE WATER (E.G. SETTLING PONDS, DETENTION PONDS, SWALES), ACTION SHALL BE TAKEN IMMEDIATELY TO REMOVE THE MATERIAL CAUSING THE SHEEN. THE CONTRACTOR SHALL USE APPROPRIATE MATERIALS TO CONTAIN AND ABSORB THE SPILL. THE SOURCE OF THE OIL SHEEN WILL ALSO BE IDENTIFIED AND REMOVED OR REPAIRED AS NECESSARY TO PREVENT FURTHER RELEASES.
- (50)

FERTILIZERS SHALL BE APPLIED ONLY IN THE AMOUNTS SPECIFIED. ONCE APPLIED, FERTILIZERS SHALL BE WORKED INTO THE SOIL TO LIMIT THE EXPOSURE TO STORMWATER.
- (51)

IF A SPILL OCCURS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR COMPLETING THE SPILL REPORTING FORM AND FOR REPORTING THE SPILL TO THE TDOT PROJECT RESPONSIBLE PARTY. ALL SPILLS MUST BE REPORTED TO THE APPROPRIATE AGENCY, AND MEASURES SHALL BE TAKEN IMMEDIATELY TO PREVENT THE POLLUTION OF WATERS OF THE STATE/U.S., INCLUDING GROUNDWATER, SHOULD A SPILL OCCUR.
- (52)

WHERE A RELEASE CONTAINING A HAZARDOUS SUBSTANCE IN AN AMOUNT EQUAL TO OR IN EXCESS OF A REPORTABLE QUANTITY ESTABLISHED UNDER EITHER 40 CFR 117 OR 40 CFR 302 OCCURS DURING A 24 HOUR PERIOD, SEE THE LATEST TENNESSEE GENERAL PERMIT NO. TNR100000 STORMWATER DISCHARGES FROM CONSTRUCTION ACTIVITIES SECTION 5.1 FOR REPORTING REQUIREMENTS.
- (53)

CONTRACTOR'S BULK FUEL AND PETROLEUM PRODUCTS STORED ONSITE OR ADJACENT TO THE R.O.W. IN ABOVE GROUND STORAGE CONTAINERS WITH A COMBINED CAPACITY OF 1320 GALLONS OR MORE SHALL HAVE SECONDARY CONTAINMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREPARING A SPILL PREVENTION CONTROL AND COUNTERMEASURE (SPCC) PLAN FOR THE BULK STORAGE AND BE SOLELY RESPONSIBLE FOR OBTAINING ANY NECESSARY LOCAL, STATE, AND FEDERAL PERMITS. THE SPCC PLAN AND/OR PERMITS SHALL BE KEPT ONSITE AND A COPY PROVIDED TO THE TDOT PROJECT RESPONSIBLE PARTY PRIOR TO STORING 1320 GALLONS ON SITE.

EROSION PREVENTION AND SEDIMENT CONTROL SPECIAL NOTES

STREAMS, WETLANDS & BUFFER ZONES

- (1)

FOR PROJECTS THAT DISCHARGE INTO KNOWN EXCEPTIONAL TENNESSEE WATERS OR WATERS IMPAIRED BY SILTATION, A 60 FOOT NATURAL RIPARIAN BUFFER ZONE ADJACENT TO AND ON BOTH SIDES OF THE RECEIVING STREAM WITH THIS DESIGNATION SHALL BE PRESERVED TO THE MAXIMUM EXTENT PRACTICABLE DURING CONSTRUCTION ACTIVITIES AT THE SITE. THE 60 FOOT CRITERION FOR THE WIDTH OF THE BUFFER ZONE CAN BE ESTABLISHED ON AN AVERAGE WIDTH BASIS AT A PROJECT, AS LONG AS THE MINIMUM WIDTH OF THE BUFFER ZONE IS MORE THAN 30 FEET AT ANY MEASURED LOCATION.
- (2)

A 30 FOOT NATURAL RIPARIAN BUFFER ZONE ADJACENT TO AND ON BOTH SIDES OF THE RECEIVING STREAM SHALL BE PRESERVED TO THE MAXIMUM EXTENT PRACTICABLE DURING CONSTRUCTION ACTIVITIES AT THE SITE. THE 30 FOOT CRITERION FOR THE WIDTH OF THE BUFFER ZONE CAN BE ESTABLISHED ON AN AVERAGE WIDTH BASIS AT A PROJECT, AS LONG AS THE MINIMUM WIDTH OF THE BUFFER ZONE IS MORE THAN 15 FEET AT ANY MEASURED LOCATION. EVERY ATTEMPT SHALL BE MADE FOR CONSTRUCTION ACTIVITIES NOT TO TAKE PLACE WITHIN THE BUFFER ZONES.
- (3)

BUFFER ZONES ARE NOT SEDIMENT CONTROL MEASURES AND MUST NOT BE RELIED UPON AS PRIMARY SEDIMENT CONTROL MEASURES. THE RIPARIAN BUFFER ZONE SHALL BE ESTABLISHED BETWEEN THE TOP OF THE STREAM BANK AND THE DISTURBED CONSTRUCTION AREA. EVERY ATTEMPT SHALL BE MADE FOR CONSTRUCTION ACTIVITIES NOT TO TAKE PLACE WITHIN THE BUFFER ZONES. BEST MANAGEMENT PRACTICES (BMPs) PROVIDING EQUIVALENT PROTECTION AS THE NATURAL RIPARIAN ZONE MAY BE USED. A JUSTIFICATION FOR USE AND DESIGN EQUIVALENCY SHALL BE DOCUMENTED WITHIN THE SWPPP. THE ENVIRONMENTAL AND ROADWAY DESIGN DIVISIONS SHALL REVIEW AND APPROVE THIS REVISION OF THE EPSC PLANS BEFORE DISTURBANCE OF THE SITE PROCEEDS, UNLESS PREVIOUSLY EXEMPT IN THE NPDES CONSTRUCTION GENERAL PERMIT. WHERE ISSUED, ARAP/401 REQUIREMENTS WILL PREVAIL IF IN CONFLICT WITH THESE BUFFER ZONE REQUIREMENTS.

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2022	76S029-M2-003	8
PS&E	2025	PROT-29(131)	9

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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

EROSION
PREVENTION &
SEDIMENT CONTROL
(EPSC) NOTES

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2022	76S029-M2-003	9
PS&E	2025	PROT-29(131)	10

TABULATED EPSC QUANTITIES						
	ITEM NO.	DESCRIPTION	UNIT	STAGE I	STAGE II	TOTAL
				QUANTITY	QUANTITY	QUANTITY
				76001-3289-14		
(1)(5)	203-01	ROAD & DRAINAGE EXCAVATION (UNCLASSIFIED)	C.Y.	7	7	14
(2)(3)	209-03.22	FILTER SOCK (18 INCH)	L.F.	320		320
(2)(3)	209-05	SEDIMENT REMOVAL	C.Y.	6		6
(2)(3)	209-40.33	CATCH BASIN PROTECTION (TYPE D)	EACH	4		4
(1)(2)(3)	709-05.05	MACHINED RIP-RAP (CLASS A-3)	TON	50	50	100
(1)(2)(3)	740-10.03	GEOTEXTILE (TYPE III)(EROSION CONTROL)	S.Y.	86	86	172
(2)(3)	740-11.02	TEMPORARY SEDIMENT TUBE 12IN	L.F.	80		80
(2)(3)(4)	801-01.07	TEMPORARY SEEDING (WITH MULCH)	UNIT		5	5
(2)(3)(4)	801-02	SEEDING (WITHOUT MULCH)	UNIT		10	10
(2)(3)(6)	801-03	WATER (SEEDING & SODDING)	M.G.		1	1
	803-01	SODDING (NEW SOD)	S.Y.		1112	1112

NOTE: ALL EROSION PREVENTION AND SEDIMENT CONTROL QUANTITIES ARE TO BE USED AS DIRECTED BY THE TDOT MANAGER.
ALL EPSC MEASURES ARE TEMPORARY AND WILL BE REMOVED AFTER CONSTRUCTION IS COMPLETED.

FOOTNOTE:

- (1) TO BE USED FOR TEMPORARY CONSTRUCTION EXITS.
- (2) ALL QUANTITIES ARE TO BE USED AS DIRECTED BY THE TDOT MANAGER.
- (3) SEE SUBSECTION 209.07 OF THE STANDARD SPECIFICATIONS FOR MAINTENANCE AND REPLACEMENT.
- (4) THE COST OF FERTILIZER AND LIME USED IN INITIAL SEED BED PREPARATION IS TO BE INCLUDED IN THE COST OF SEEDING. SEE SECTION 801 OF TDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.
- (5) SEE GRADING SPECIAL NOTES ON SHEET 2D.
- (6) INCLUDES 1 THOUSAND GALLONS FOR EROSION PREVENTION AND SEDIMENT CONTROL.
- (7) INCLUDES 19 C.Y. TO BE USED FOR SPECIAL DITCH

EROSION PREVENTION AND SEDIMENT CONTROL LEGEND		
SYMBOL	ITEM	STD. DWG.
* *SOCK 18" * *SOCK 18" * *	18 INCH FILTER SOCK	EC-STR-8
	CATCH BASIN PROTECTION (TYPE D)	EC-STR-19
	TEMPORARY CONSTRUCTION EXIT	EC-STR-25
* *TUBE 12" * *TUBE 12" * *	12 INCH SEDIMENT TUBE	EC-STR-37
	SOD	

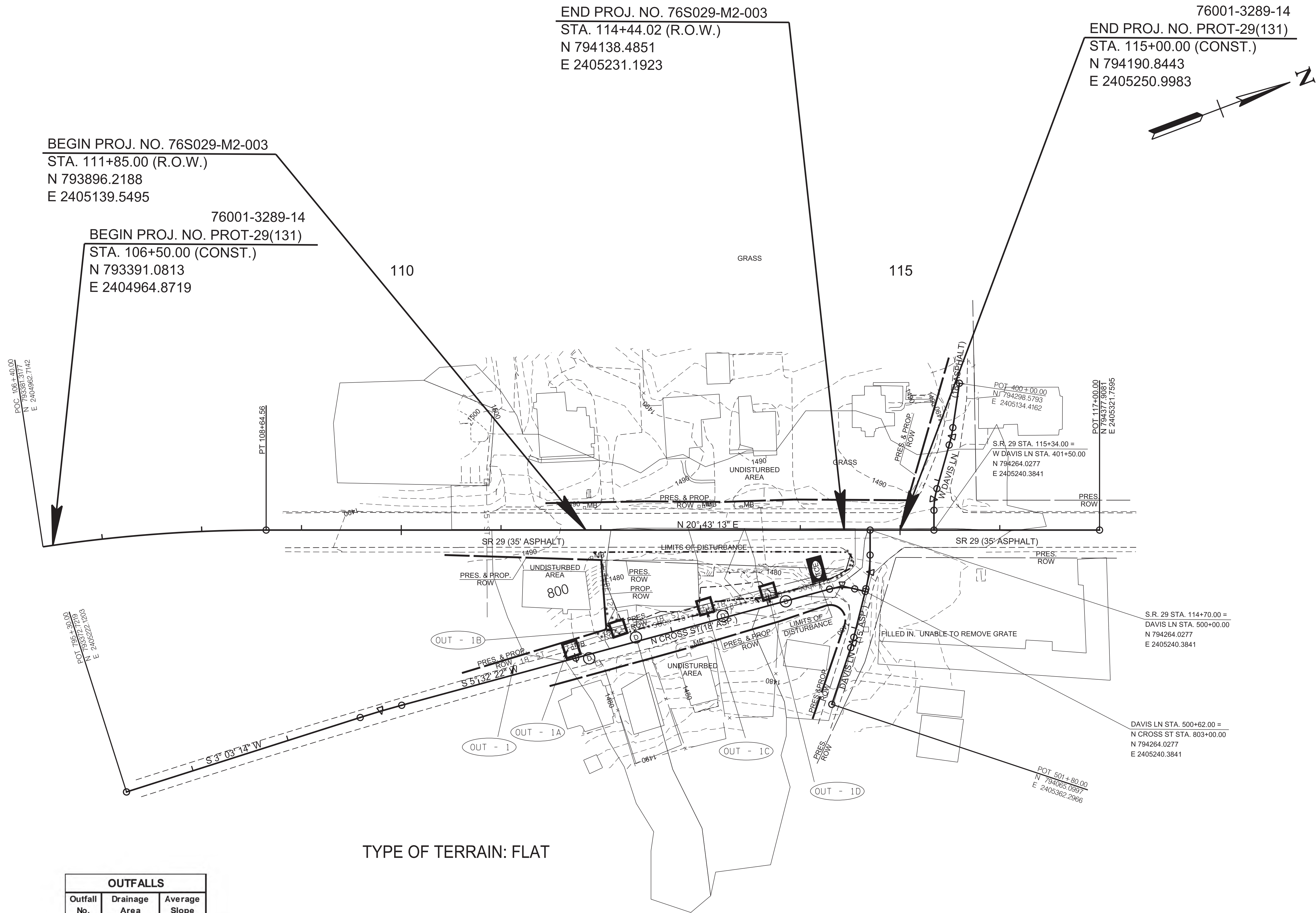
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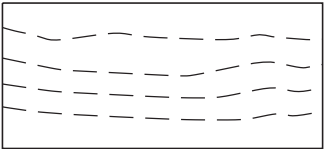
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

EROSION PREVENTION & SEDIMENT CONTROL (EPSC) LEGEND & TABULATION

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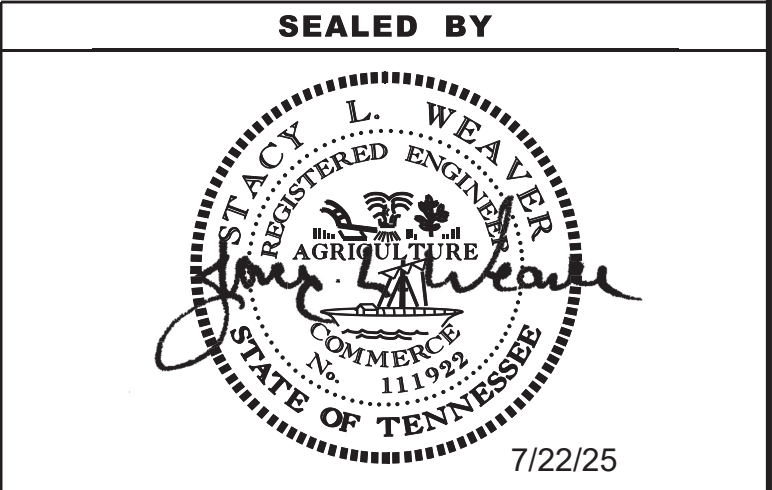


OUTFALLS		
Outfall No.	Drainage Area	Average Slope
1	1.43 AC	.8 %
1A	.31 AC	8.2 %
1B	.91 AC	11.7 %
1C	.07 AC	15.2 %
1D	.01 AC	6.6 %



EXISTING
CONTOURS

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2022	76S029-M2-003	10
PS&E	2025	PROT-29(131)	11



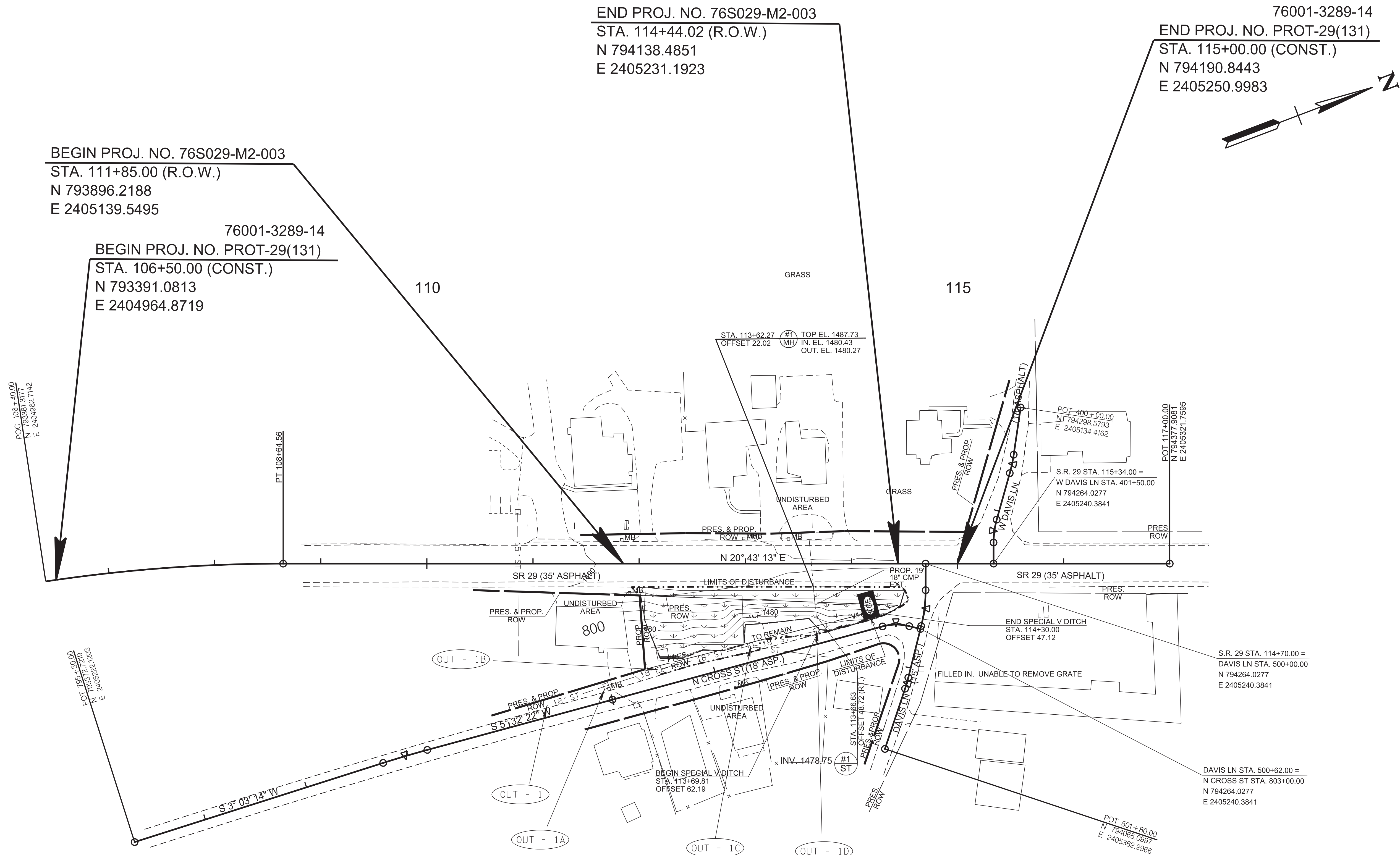
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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

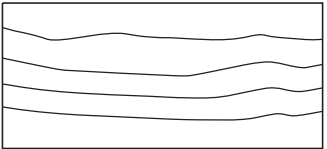
EROSION
PREVENTION &
SEDIMENT CONTROL
(EPSC) PLANS
STA. 106+50 TO STA. 115+00
SCALE: 1"= 50'

STAGE I

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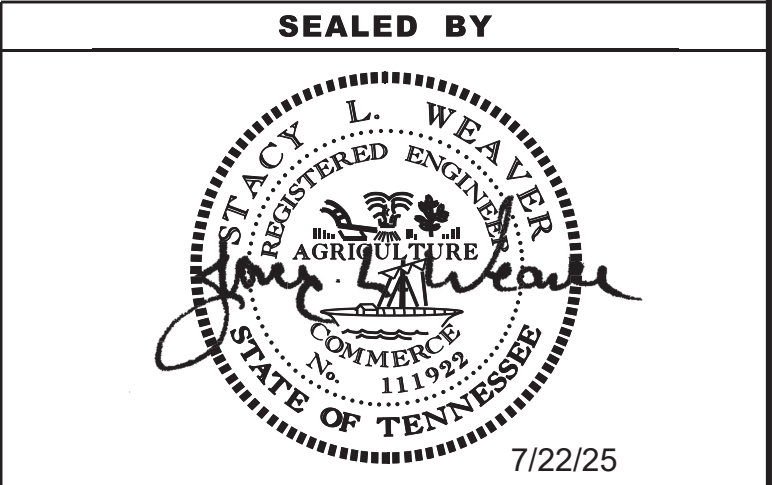


OUTFALLS		
Outfall No.	Drainage Area	Average Slope
1	1.43 AC	.8 %
1A	.31 AC	8.2 %
1B	.91 AC	11.7 %
1C	.07 AC	15.2 %
1D	.01 AC	6.6 %



STAGE II
PROPOSED
CONTOURS

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2022	76S029-M2-003	11
PS&E	2025	PROT-29(131)	12

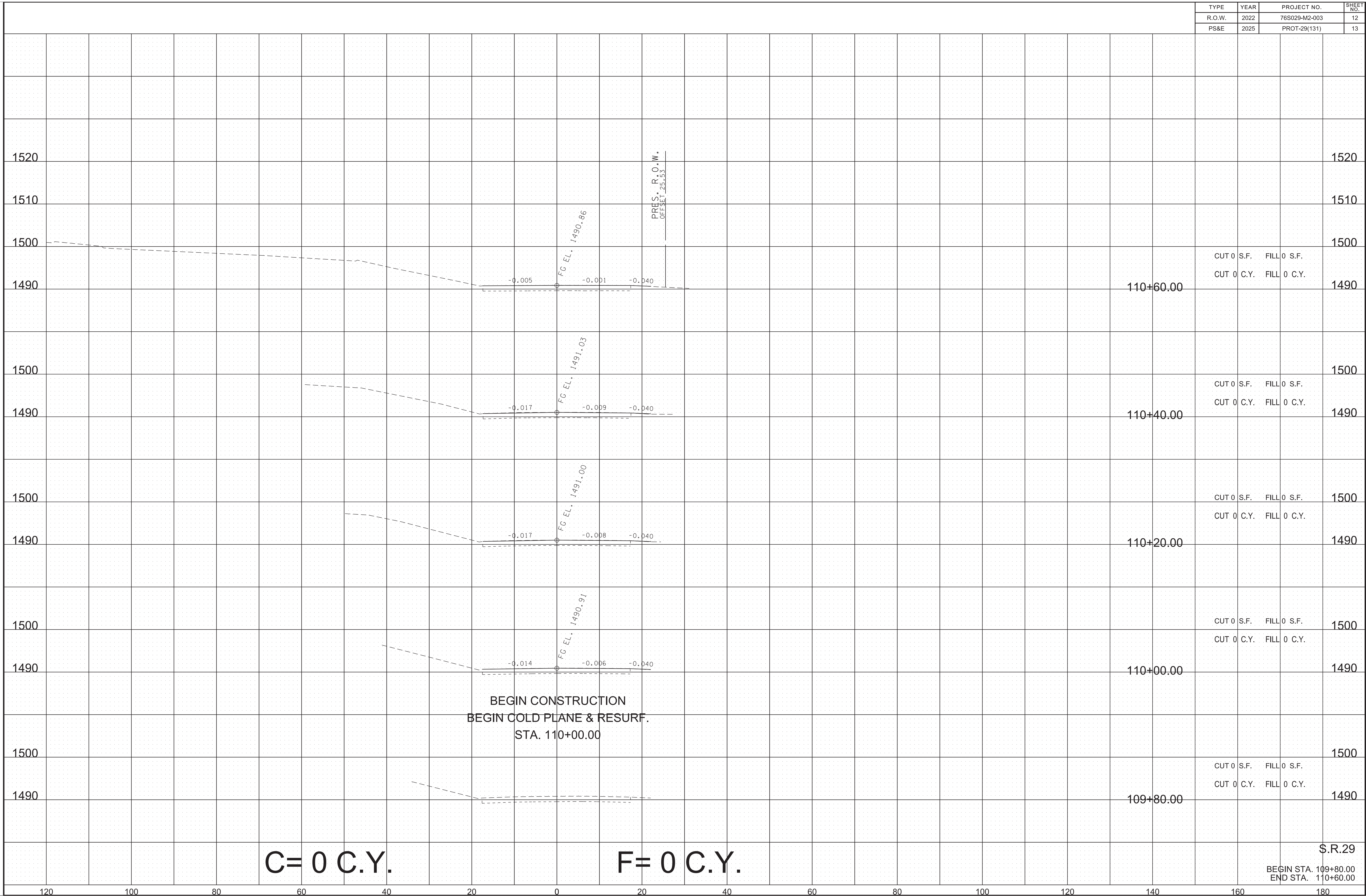


COORDINATES ARE NAD 83(2011), ARE DATUM ADJUSTED BY THE FACTOR OF 1.00005 AND TIED TO THE TGRN. ALL ELEVATIONS ARE REFERENCED TO THE NAVD 1988 WITH GEOID 18.

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

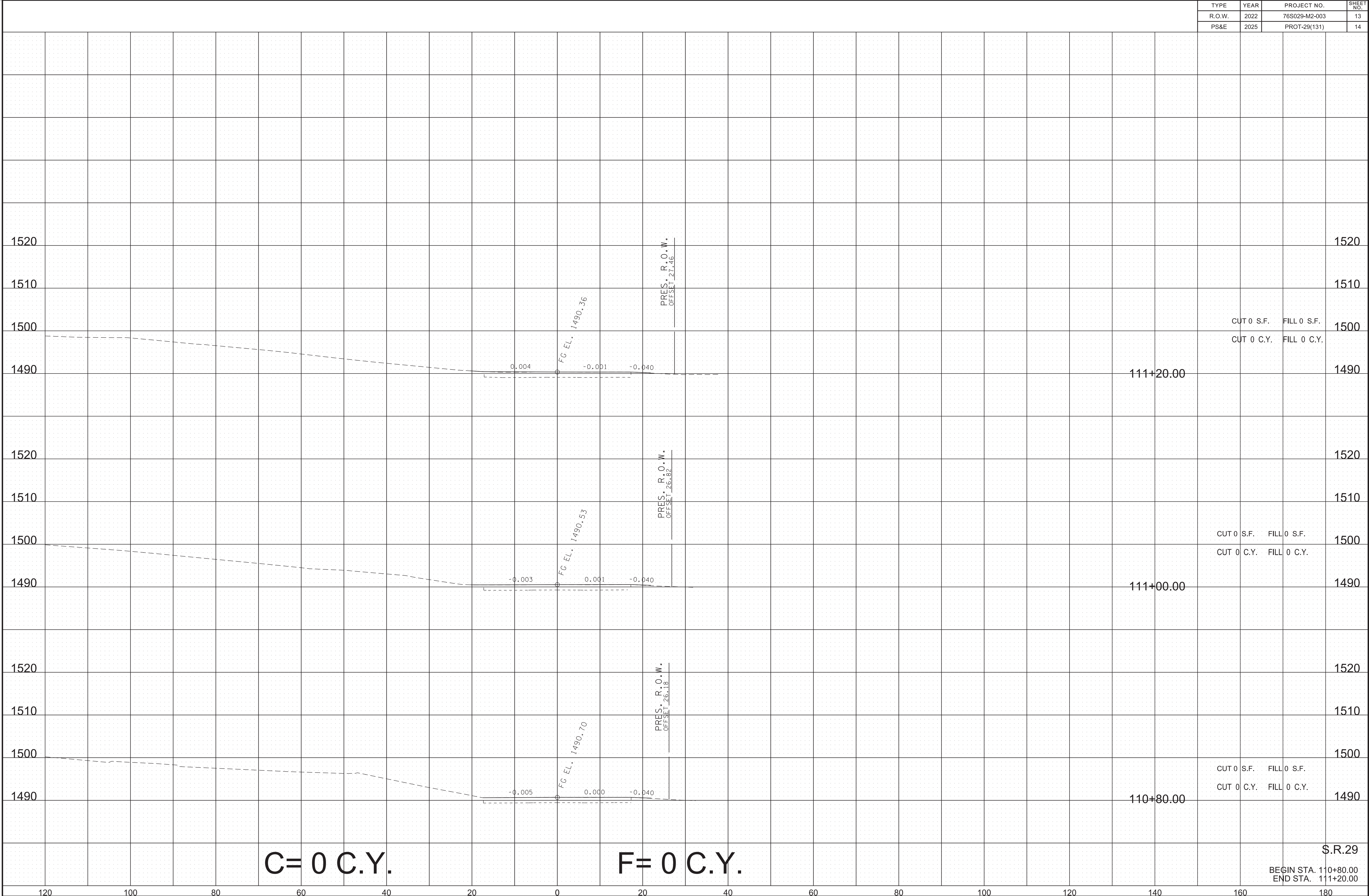
EROSION
PREVENTION &
SEDIMENT CONTROL
(EPSC) PLANS
STA. 106+50 TO STA. 115+00
SCALE: 1"= 50'

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2022	76S029-M2-003	12
PS&E	2025	PROT-29(131)	13



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TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2022	76S029-M2-003	13
PS&E	2025	PROT-29(131)	14



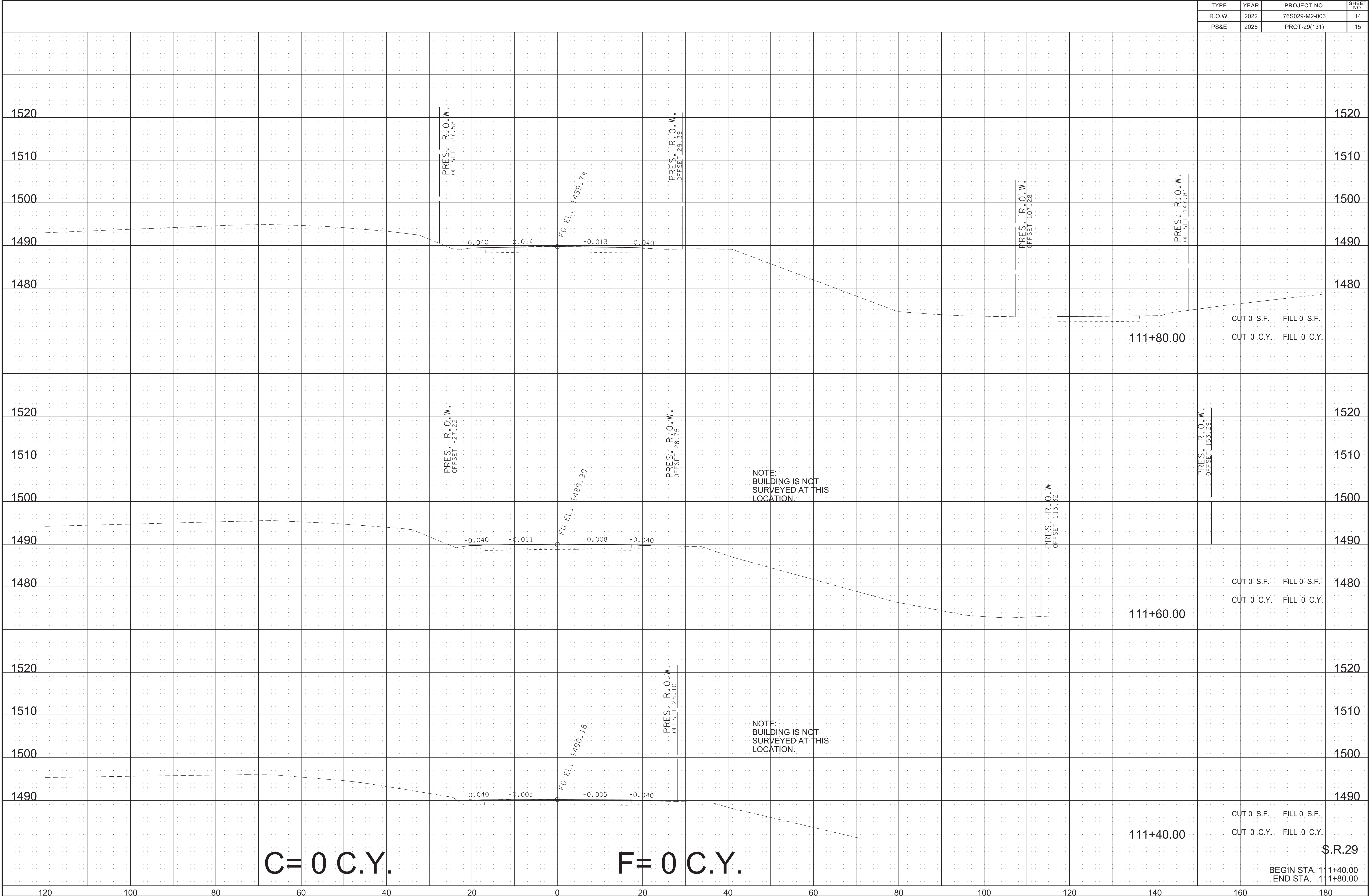
C= 0 C.Y.

F= 0 C.Y.

S.R.29

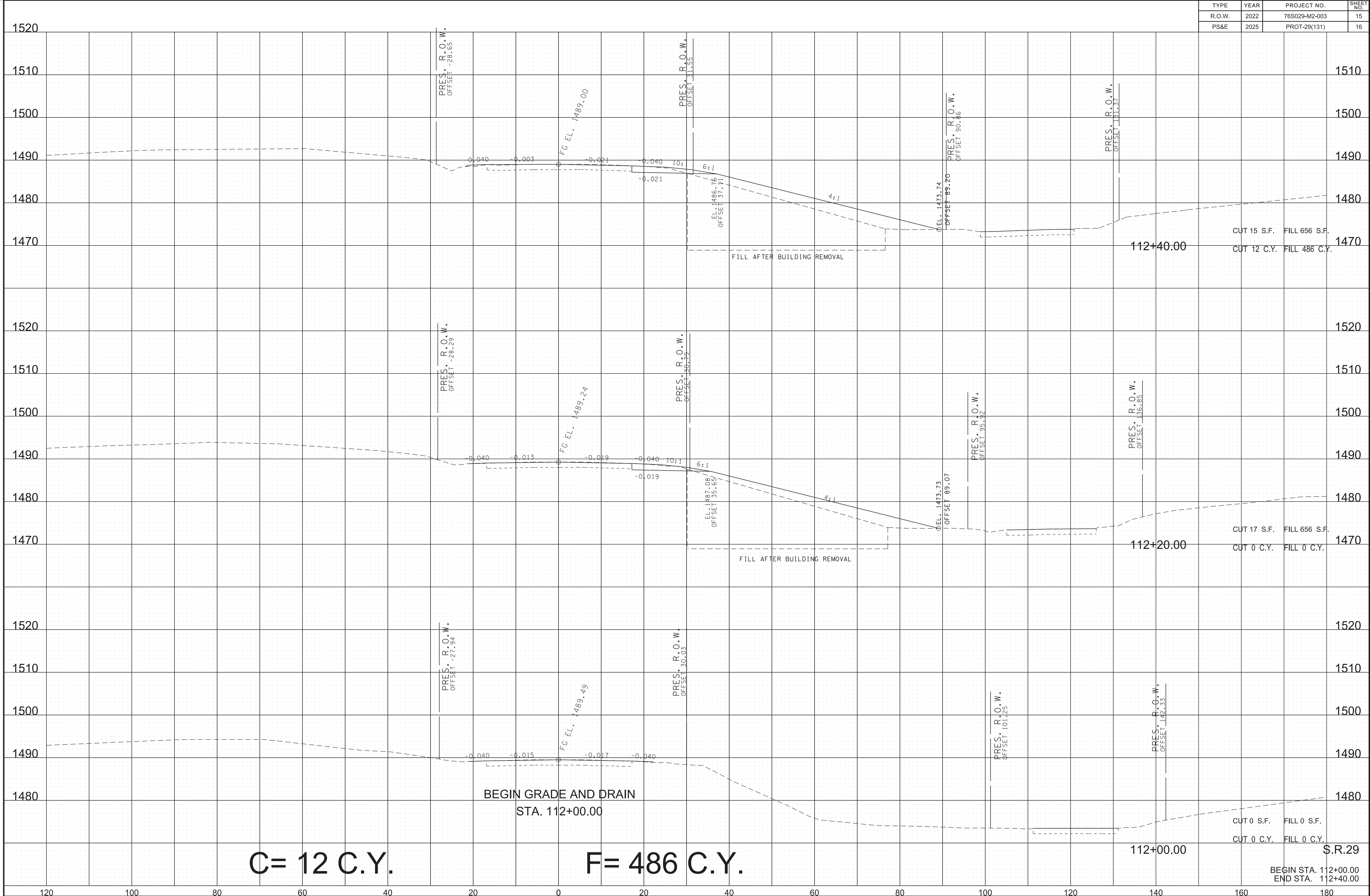
BEGIN STA. 110+80.00
END STA. 111+20.00

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2022	76S029-M2-003	14
PS&E	2025	PROT-29(131)	15



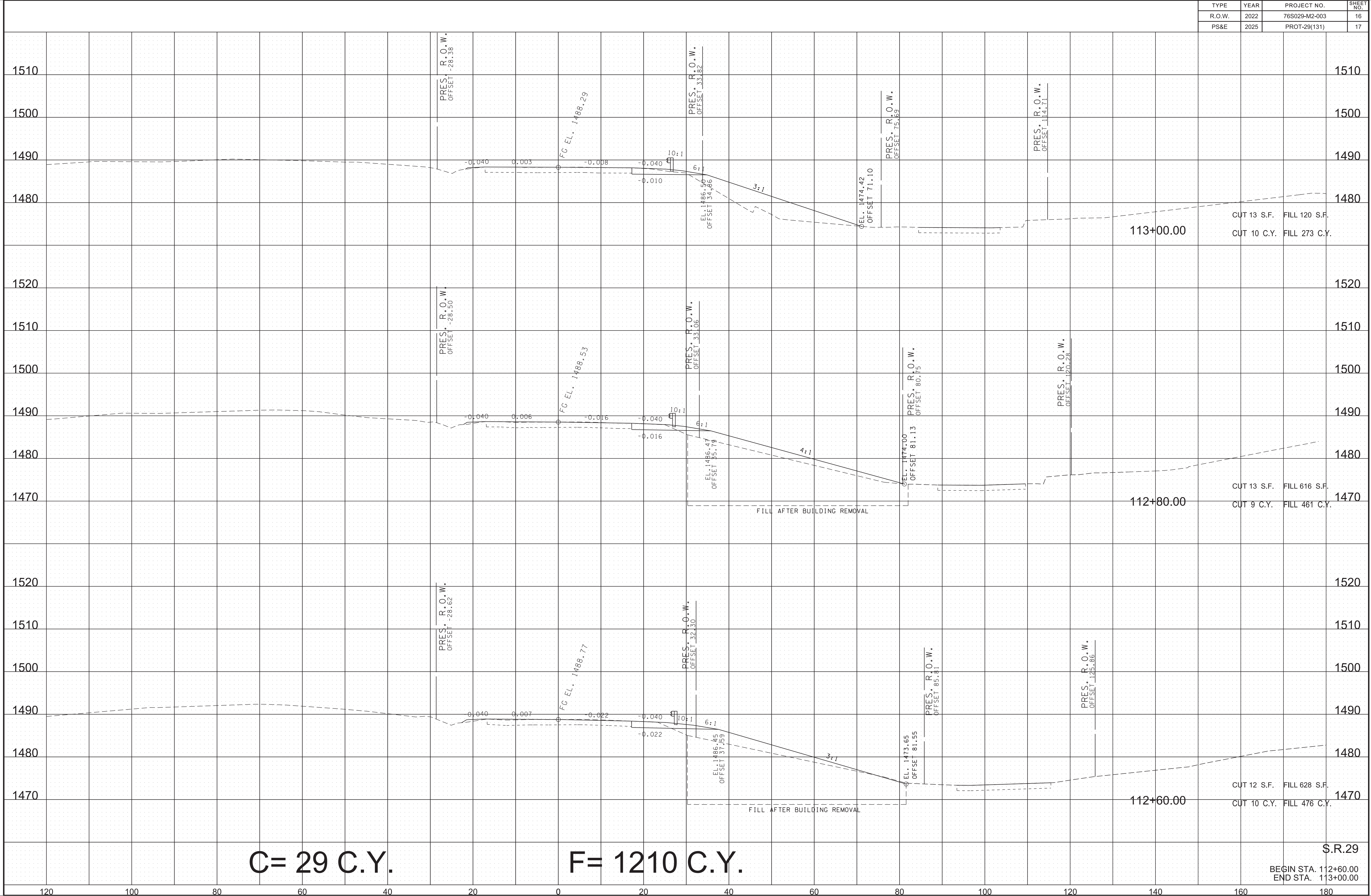
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TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2022	76S029-M2-003	15
PS&E	2025	PROT-29(131)	16

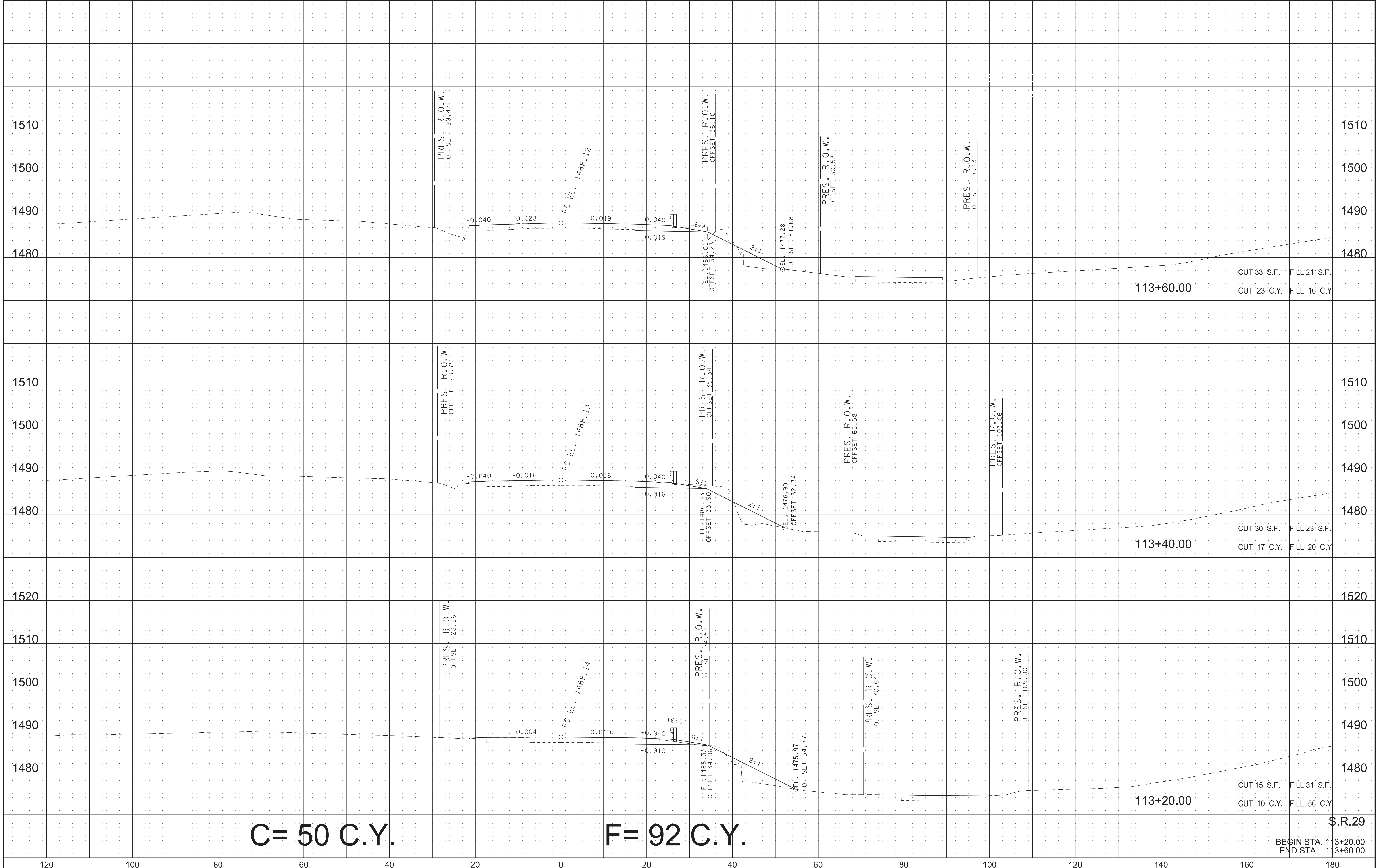


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TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2022	76S029-M2-003	16
PS&E	2025	PROT-29(131)	17

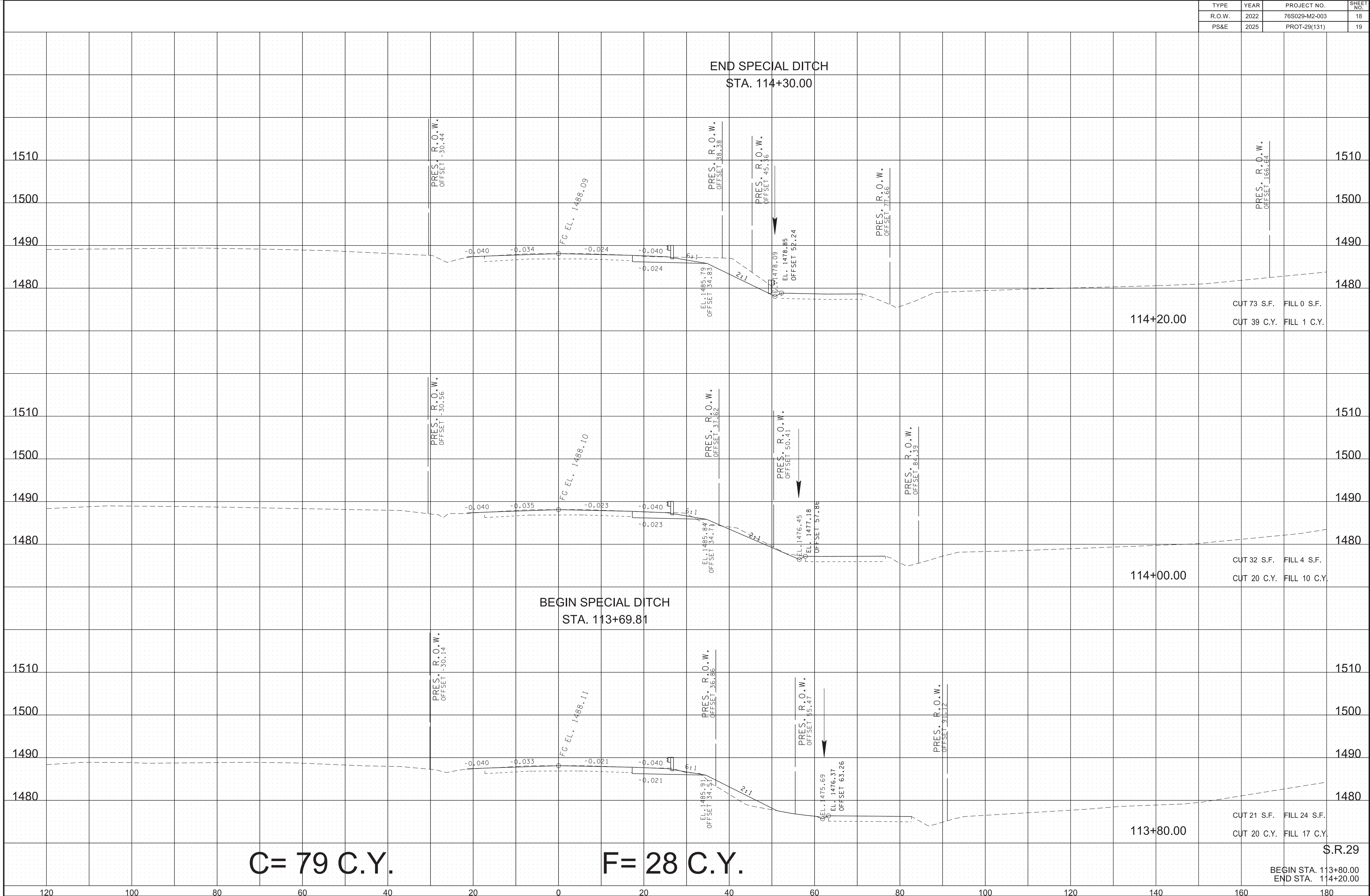


TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2022	76S029-M2-003	17
PS&E	2025	PROT-29(131)	18



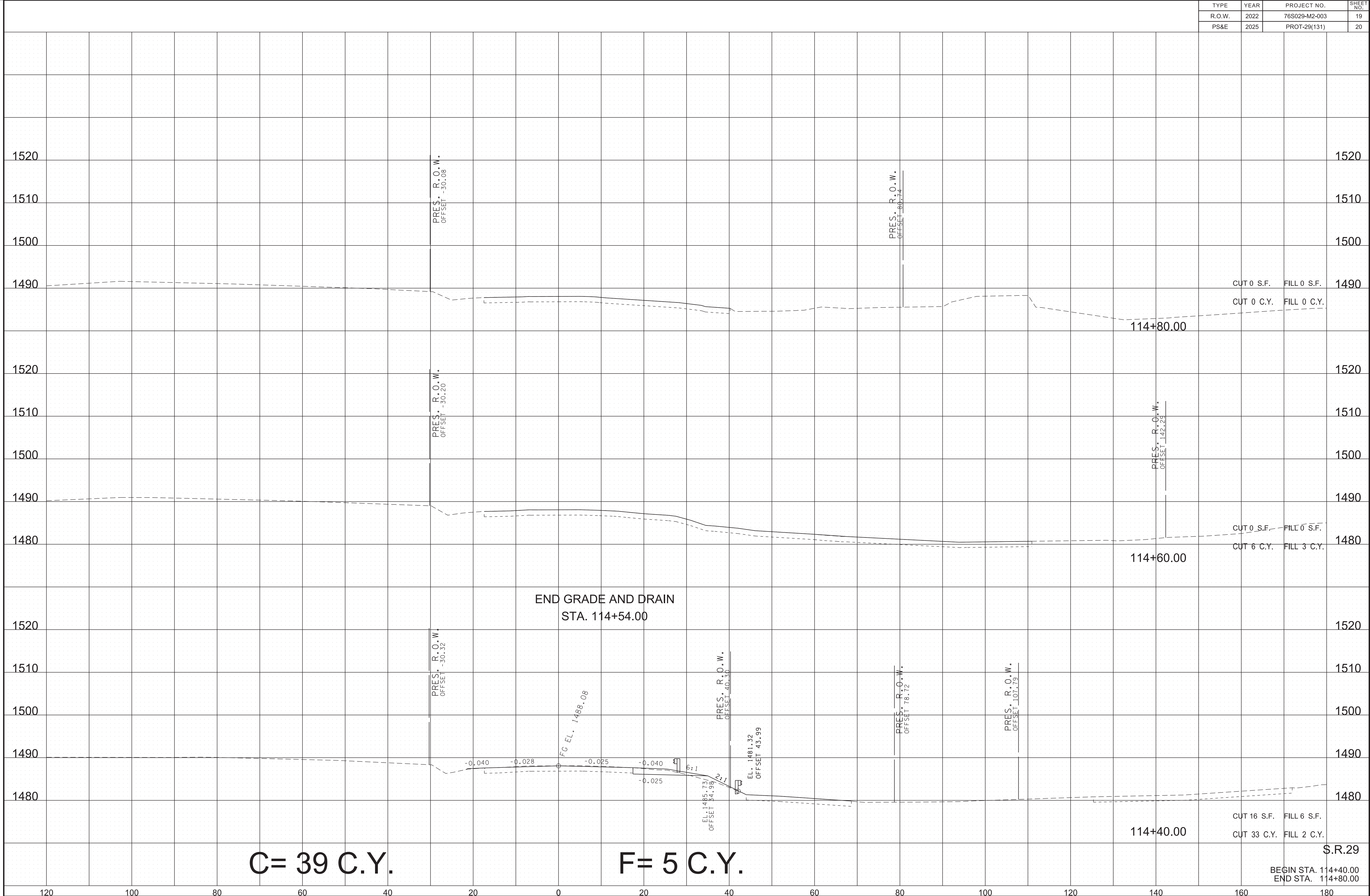
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TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2022	76S029-M2-003	18
PS&E	2025	PROT-29(131)	19



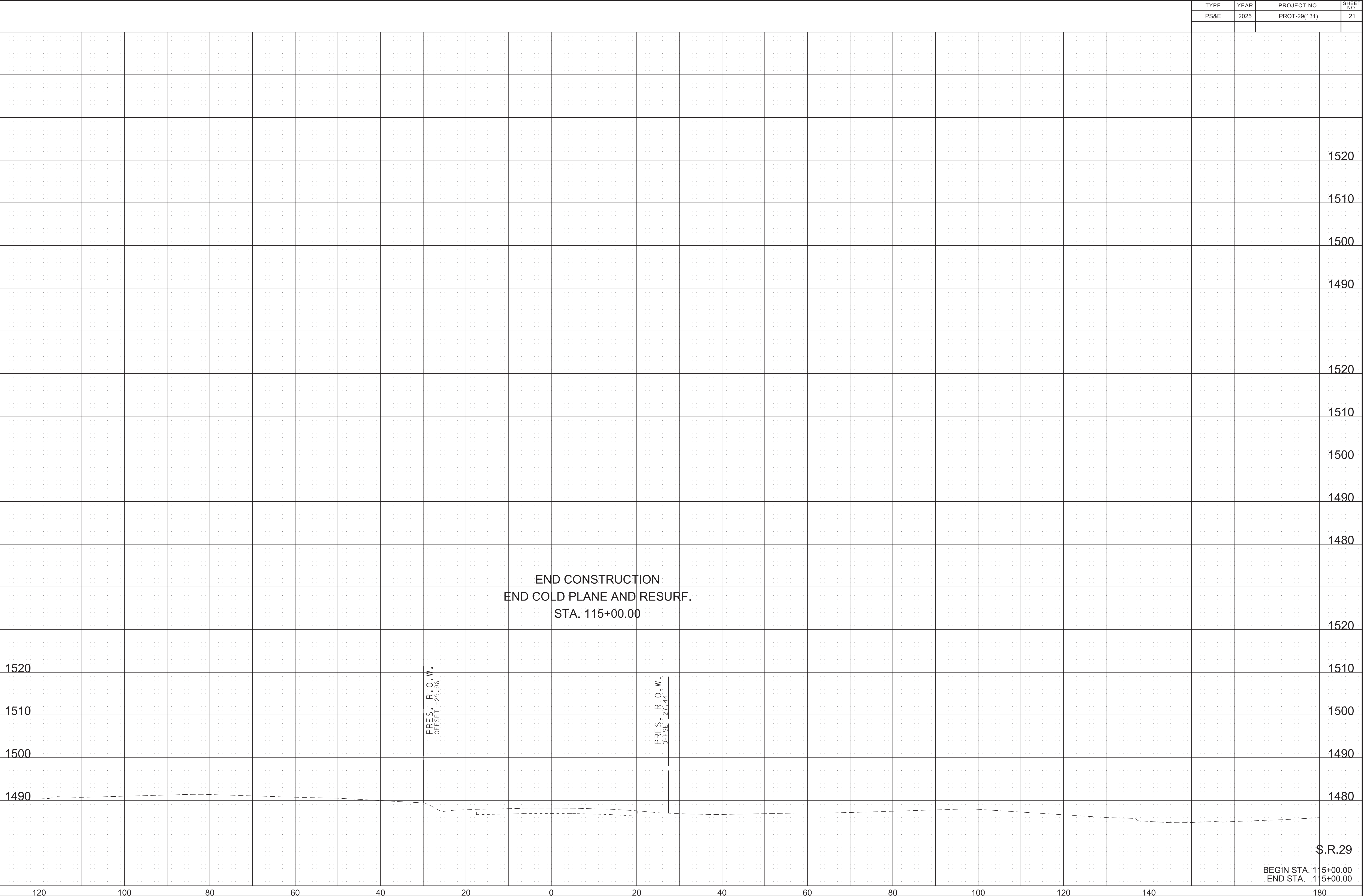
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TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2022	76S029-M2-003	19
PS&E	2025	PROT-29(131)	20



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TYPE	YEAR	PROJECT NO.	SHEET NO.
PS&E	2025	PROT-29(131)	21



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PAVEMENT EDGE DROP-OFF TRAFFIC CONTROL NOTES

A. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES OR TRAFFIC LANE AND SHOULDER WHERE THE TRAFFIC LANE IS BEING USED BY TRAFFIC, CAUSED BY BASE, PAVING OR RESURFACING:

1. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 0.75 INCH AND NOT EXCEEDING 1.75 INCHES:

a. WARNING SIGNS, UNEVEN LANES (W8-11) AND/OR SHOULDER DROP-OFF WITH PLAQUE (W8-17 AND W8-17P), SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

b. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY ADDED PAVEMENT SHALL BE ELIMINATED WITHIN THREE WORKDAYS.

c. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY COLD PLANING SHALL BE ELIMINATED WITHIN THREE WORKDAYS.

d. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE TRAFFIC LANE BEING UTILIZED BY TRAFFIC AND SHOULDER THE DIFFERENCE IN ELEVATION SHALL BE ELIMINATED WITHIN SEVEN WORKDAYS AFTER THE CONDITION IS CREATED.
2. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 1.75 INCHES AND NOT EXCEEDING 6 INCHES, TRAFFIC IS NOT TO BE ALLOWED TO TRAVERSE THIS DIFFERENCE IN ELEVATION.

a. SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

(1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

(2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

b. IF THE DIFFERENCE IN ELEVATION IS ELIMINATED OR DECREASED TO 2 INCHES OR LESS BY THE END OF EACH WORKDAY, CONES MAY BE USED DURING DAYLIGHT HOURS IN LIEU OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES MENTIONED IN PARAGRAPH a, PROVIDED WARNING SIGNS ARE ERECTED. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.
- c. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE THROUGH TRAFFIC LANE AND THE SHOULDER AND THE ELEVATION DIFFERENCE IS LESS THAN 3 INCHES, THE CONTRACTOR MAY USE WARNING SIGNS AND/OR PROTECTIVE DEVICES AS APPLICABLE AND APPROVED BY THE REGIONAL TRAFFIC ENGINEER. SEE PARAGRAPH a REGARDING USE OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) WILL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

IN THESE SITUATIONS, THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 2 MILES IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

3. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 6 INCHES BUT NOT EXCEEDING 18 INCHES, THE CONTRACTOR, WITH THE ENGINEER'S APPROVAL, MAY UTILIZE ONE OF THE FOLLOWING:

a. THE CONTRACTOR SHALL ACCOMPLISH SEPARATION BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

(1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

(2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.
- IN ORDER TO USE THIS METHOD, THE CONTRACTOR MUST REDUCE THE DIFFERENCE IN ELEVATION TO 6 INCHES OR LESS BY THE END OF THE WORKDAY THAT THE CONDITION IS CREATED.
- b. THE CONTRACTOR SHALL PROVIDE DRUMS, BARRICADES OR OTHER APPROVED SEPARATION DEVICES AS SPECIFIED IN PARAGRAPH a, AND CONSTRUCT A STONE WEDGE WITH A 4:1 SLOPE, OR FLATTER, TO ELIMINATE THE VERTICAL OFFSET IF THE LOWER ELEVATION IS AT OR BELOW SUBGRADE AT THE END OF EACH DAY.

c. THE CONTRACTOR SHALL PROVIDE DRUMS, BARRICADES OR OTHER APPROVED SEPARATION DEVICES AS SPECIFIED IN PARAGRAPH a AND IF THE LOWER ELEVATION IS BASE STONE OR ASPHALT PAVEMENT, PLACEMENT OF SUBSEQUENT LAYERS OF PAVEMENT MUST BEGIN THE NEXT WORK DAY AND PROGRESS CONTINUOUSLY UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED OR REDUCED TO SIX INCHES OR LESS.

d. THE CONTRACTOR SHALL PROVIDE SEPARATION BY PORTABLE BARRIER RAIL.
- FOR PRECEDING CONDITIONS a, b, AND c, THE CONTRACTOR SHALL USE THE SHOULDER DROP-OFF WARNING SIGN WITH PLAQUE (W8-17 AND W8-17P). IT SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN THE SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. IN THESE SITUATIONS, THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 1 MILE IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.
4. FOR DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 18 INCHES.
- SEPARATION WILL BE PROVIDED BY USE OF PORTABLE BARRIER RAIL.
- IN THIS SITUATION THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 1 MILE IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.
- B. IF THE DIFFERENCE IN ELEVATION IS WITHIN 30 FEET OF THE NEAREST TRAFFIC LANE BEING USED BY TRAFFIC CAUSED BY GRADING, EXCAVATION FOR UTILITIES, DRAINAGE STRUCTURES, UNDERCUTTING, ETC.:
1. IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 0.75 INCH AND NOT EXCEEDING 2 INCHES.

a. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

2. IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 2 INCHES AND NOT EXCEEDING 6 INCHES:

a. SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

(1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

(2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

3. IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 6 INCHES:


a. SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

(1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

(2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

b. ELIMINATE VERTICAL OFFSET BY CONSTRUCTING A STONE WEDGE OR GRADING TO A 4:1 SLOPE, OR FLATTER, OR USE PORTABLE BARRIER RAIL.
- THE CONTRACTOR SHALL SCHEDULE THE WORK SO AS TO MINIMIZE THE TIME TRAFFIC IS EXPOSED TO AN ELEVATION DIFFERENCE. ONCE THE CONTRACTOR BEGINS AN ACTIVITY THAT CREATES AN ELEVATION DIFFERENCE WITHIN 8 FEET OF A TRAFFIC LANE, THE ACTIVITY SHALL BE PURSUED AS A CONTINUOUS OPERATION UNTIL THE ELEVATION DIFFERENCE IS ELIMINATED.
- C. IF THE DIFFERENCE IN ELEVATION IS FARTHER THAN 8 FEET FROM THE NEAREST TRAFFIC LANE BUT NOT MORE THAN 30 FEET FROM THE NEAREST TRAFFIC LANE:
- SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:
1. WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

2. WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.
- THE CONTRACTOR SHALL SCHEDULE THE WORK SO AS TO MINIMIZE THE TIME TRAFFIC IS EXPOSED TO AN ELEVATION DIFFERENCE. ONCE THE CONTRACTOR BEGINS AN ACTIVITY THAT CREATES AN ELEVATION DIFFERENCE, THE ACTIVITY SHALL BE PURSUED AS A CONTINUOUS OPERATION UNTIL THE ELEVATION DIFFERENCE IS ELIMINATED.
- | | | | |
|------|------|--------------|-----------|
| TYPE | YEAR | PROJECT NO. | SHEET NO. |
| PS&E | 2025 | PROT-29(131) | T1 |
| | | | |
| | | | |
- SEALED BY



7/22/25
- STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PAVEMENT EDGE
DROP-OFF NOTES
FOR
TRAFFIC CONTROL

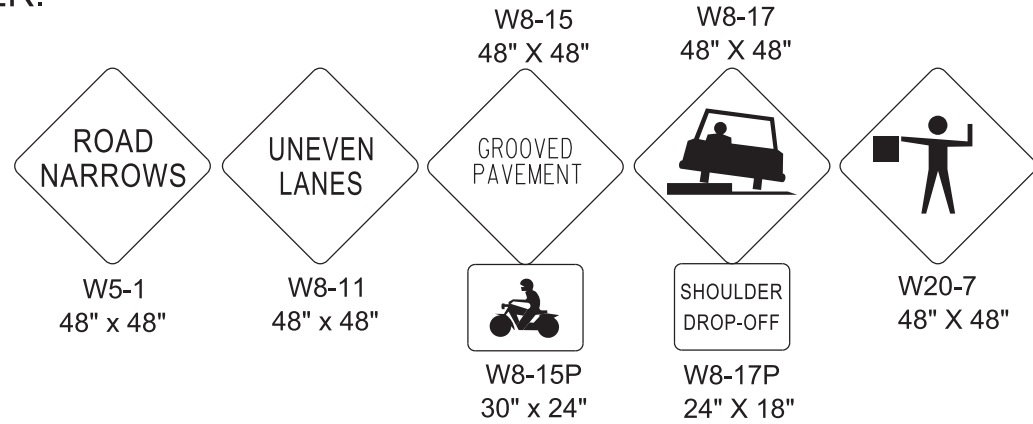
TRAFFIC CONTROL NOTES

THE CONSTRUCTION SIGNING PLANS ARE TO SERVE AS A GUIDE ONLY.
OTHER SIGNS MAY BE REQUIRED DURING VARIOUS PHASES OF CONSTRUCTION.

THIS TRAFFIC CONTROL PLAN DOES NOT RELIEVE THE CONTRACTOR OF THE
RESPONSIBILITY OF INSTALLING TRAFFIC CONTROL DEVICES IN ACCORDANCE WITH
THE CURRENT EDITION OF THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES."

ALL CONSTRUCTION SIGNS ON THE PROJECT SHALL BE COVERED WHEN WORK WILL
NOT BE AFFECTING TRAFFIC. COST OF COVERING/UNCOVERING SIGNS SHALL
NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COST SHALL BE
INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO. 712-06,
SIGNS (CONSTRUCTION) PER SQUARE FOOT.

ALL TRAFFIC CONTROL DEVICES SHALL BE APPROVED BY TDOT OPERATIONS
MANAGER.



SIGNS SHOWN ABOVE ARE TO BE USED WHEN CONSTRUCTION OPERATIONS WARRANT.
TO BE USED AS DIRECTED BY THE TDOT MANAGER.

NOTE:

TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
	WORK ZONE
	FLEXIBLE DRUMS (CHANNELIZING)
	SIGN (CONSTRUCTION)
	TRAFFIC FLOW
	PORTABLE BARRIER RAIL (WITH BARRIER RAIL DELINEATORS)
	TEMPORARY CRASH CUSHION
	BARRICADE

TRAFFIC CONTROL PHASING NOTES

(BASED ON STD. DWG. NOS. T-WZ-10 & T-WZ-16)

PHASE 1

- (1) CLOSE NORTH CROSS STREET BETWEEN CHURCH STREET AND DAVIS LANE, ALLOWING ACCESS ONLY FROM CHURCH STREET FOR LOCAL TRAFFIC. SHIFT TRAFFIC FROM THE RIGHT (NORTHBOUND) TRAVEL LANE TO THE MIDDLE TURN LANE, AND MAINTAIN TRAFFIC ON S.R. 29 WITH TWO 11-FOOT TRAVEL LANES.
- (2) PROTECT THE WORK AREA WITH PORTABLE BARRIER RAIL.
- (3) REMOVE THE TRACT 3 BUILDING.
- (4) PERFORM SLOPE REPAIR AND WIDEN S.R. 29 RIGHT SHOULDER AS SHOWN ON SHEET NO. T4.

PHASE 2

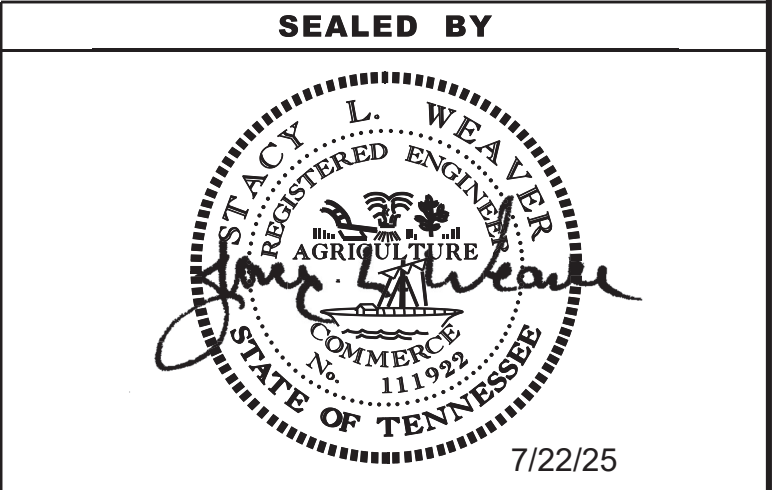
- (1) REMOVE TRAFFIC CONTROL ON S.R. 29 AND NORTH CROSS STREET.
- (2) RESURFACE S.R. 29 AS INDICATED AS ON THE ROADWAY TYPICAL SECTIONS.
- (3) APPLY FINAL PAVEMENT MARKINGS.

TABULATED TRAFFIC CONTROL QUANTITIES			
ITEM NO.	DESCRIPTION	UNIT	QUANTITY
			76001-3289-14
712-01	TRAFFIC CONTROL	LS	1
712-02.02	INTERCONNECTED PORTABLE BARRIER RAIL	L.F.	370
712-02.60	TEMPORARY WORK ZONE CRASH CUSHION (MASH TL-3)	EACH	4
712-04.01	FLEXIBLE DRUMS (CHANNELIZING)	EACH	60
712-04.50	BARRIER RAIL DELINEATOR	EACH	23
712-06	SIGNS (CONSTRUCTION)	S.F.	842
712-07.03	TEMPORARY BARRICADES (TYPE III)	L.F.	36
712-09.04	REMOVABLE PAVEMENT MARKING (STOP LINE)	L.F.	35
716-05.01	PAINTED PAVEMENT MARKING (4" LINE)	L.M.	0.6
716-08.20	REMOVAL OF PAVEMENT MARKING (LINE)	L.M.	0.2

TRAFFIC CONTROL SIGN TABULATION

M.U.T.C.D. SIGN NO.	LEGEND	SIZE IN INCHES			S.F.	NO. REQUIRED PHASE I	NO. REQUIRED PHASE II	TOTAL NO. REQUIRED	ITEM NO. 712-06 S.F.	STANDARD DRAWING NO.	REMARKS
G20-1	ROAD WORK NEXT 1 MILE	48"	X	24"	8.00	2	2	4	32.00	T-WZ-10, T-WZ-16	
G20-2	END ROADWORK	48"	X	24"	8.00	2	2	4	32.00	T-WZ-10, T-WZ-16	
M4-8	DETOUR	24"	X	12"	2.00	5	4	9	18.00		
M5-1L	ADVANCE TURN ARROW	21"	X	15"	2.19	2	2	4	8.75		
M5-1R	ADVANCE TURN ARROW	21"	X	15"	2.19	2	2	4	8.75		
MOD	N CROSS STREET	30"	X	24"	5.00	7	2	9	45.00		
R11-2	ROAD CLOSED	48"	X	30"	10.00	1	1	2	20.00		
R11-4	ROAD CLOSED AHEAD LOCAL TRAFFIC ONLY	60"	X	30"	12.50	1	1	2	25.00		
W1-4L	REVERSE CURVE	48"	X	48"	16.00	1	1	2	32.00	T-WZ-16	
W1-4R	REVERSE CURVE	48"	X	48"	16.00	1	1	2	32.00	T-WZ-16	
W5-1	ROAD NARROWS	48"	X	48"	16.00	2	2	4	64.00		
W8-11	UNEVEN LANES	48"	X	48"	16.00	2	2	4	64.00		
W8-15	GROOVED PAVEMENT	48"	X	48"	16.00	2	2	4	64.00		
W8-15P	MOTORCYCLE SYMBOL	30"	X	24"	5.00	2	2	4	20.00		
W8-17	SHOULDER DROP-OFF SYMBOL	48"	X	48"	16.00	2	2	4	64.00		
W8-17P	SHOULDER DROP-OFF PLAQUE	30"	X	24"	5.00	2	2	4	20.00		
W20-1	ROAD WORK AHEAD	48"	X	48"	16.00	2	2	4	64.00	T-WZ-10	
W20-1	ROAD WORK 1000 FT	48"	X	48"	16.00	2	2	4	64.00	T-WZ-10	
W20-1	ROAD WORK 1500 FT	48"	X	48"	16.00	1	1	2	32.00	T-WZ-10	
W20-1	ROAD WORK 500 FT	48"	X	48"	16.00	1	1	2	32.00	T-WZ-10	
W20-2	DETOUR AHEAD	36"	X	36"	9.00	2	2	4	36.00		
W20-7	FLAGGER	48"	X	48"	16.00	2	2	4	64.00		
TOTAL									842	S.F.	

TYPE	YEAR	PROJECT NO.	SHEET NO.
PS&E	2025	PROT-29(131)	T2

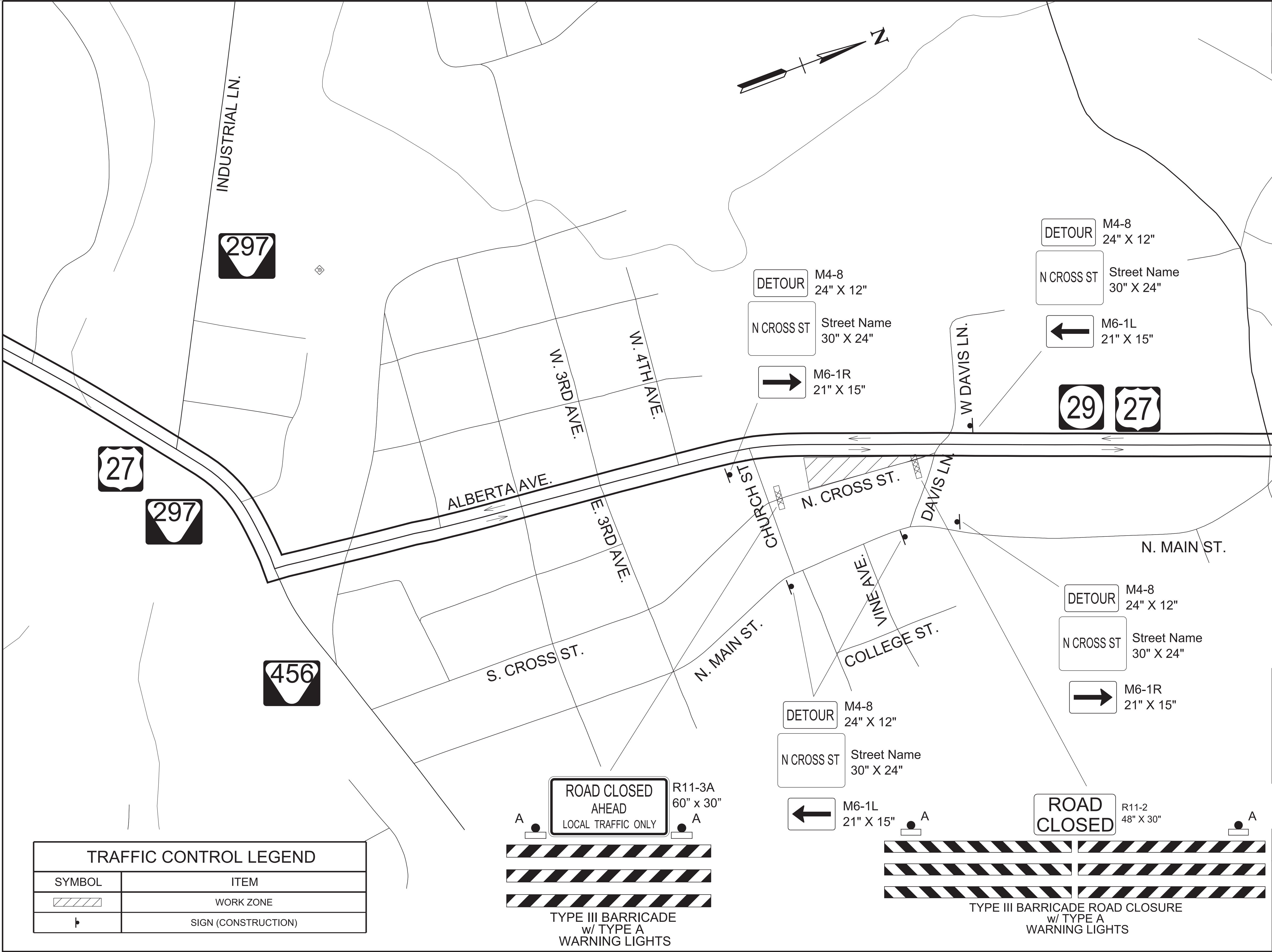


STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL
PHASING NOTES,
LEGEND AND
TABULATION

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TYPE	YEAR	PROJECT NO.	SHEET NO.
PS&E	2025	PROT-29(131)	T3



TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
	WORK ZONE
	SIGN (CONSTRUCTION)

A

ROAD CLOSED
AHEAD
LOCAL TRAFFIC ONLY

A

TYPE III BARRICADE
w/ TYPE A
WARNING LIGHTS

R11-3A
60" x 30"

A

←

A

M6-1L
21" X 15"

TYPE III BARRICADE ROAD CLOSURE
w/ TYPE A
WARNING LIGHTS

ROAD
CLOSED

R11-2
48" X 30"

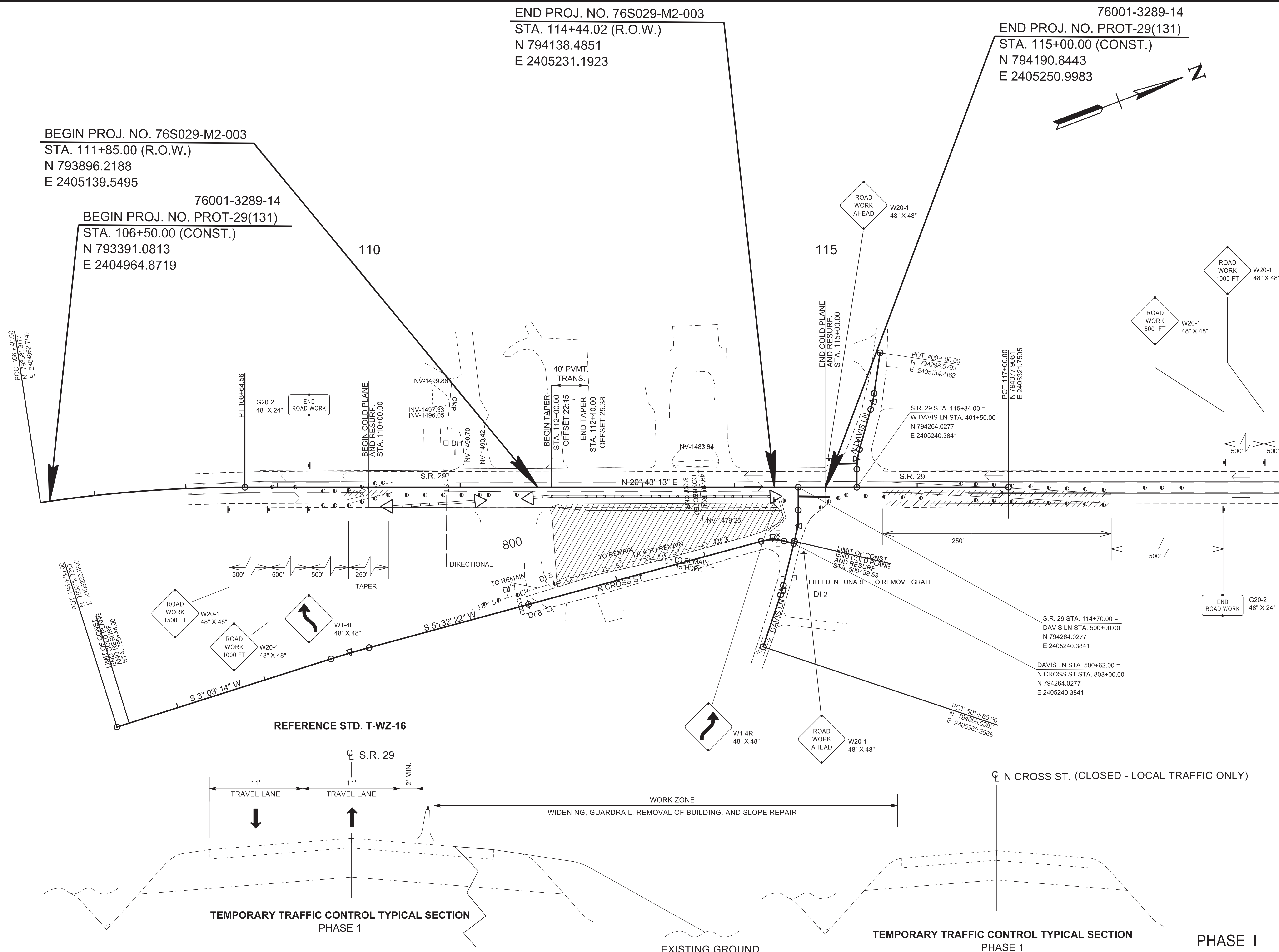
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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

DETOUR
MAP

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TYPE	YEAR	PROJECT NO.	SHEET NO.
PS&E	2025	PROT-29(131)	T4

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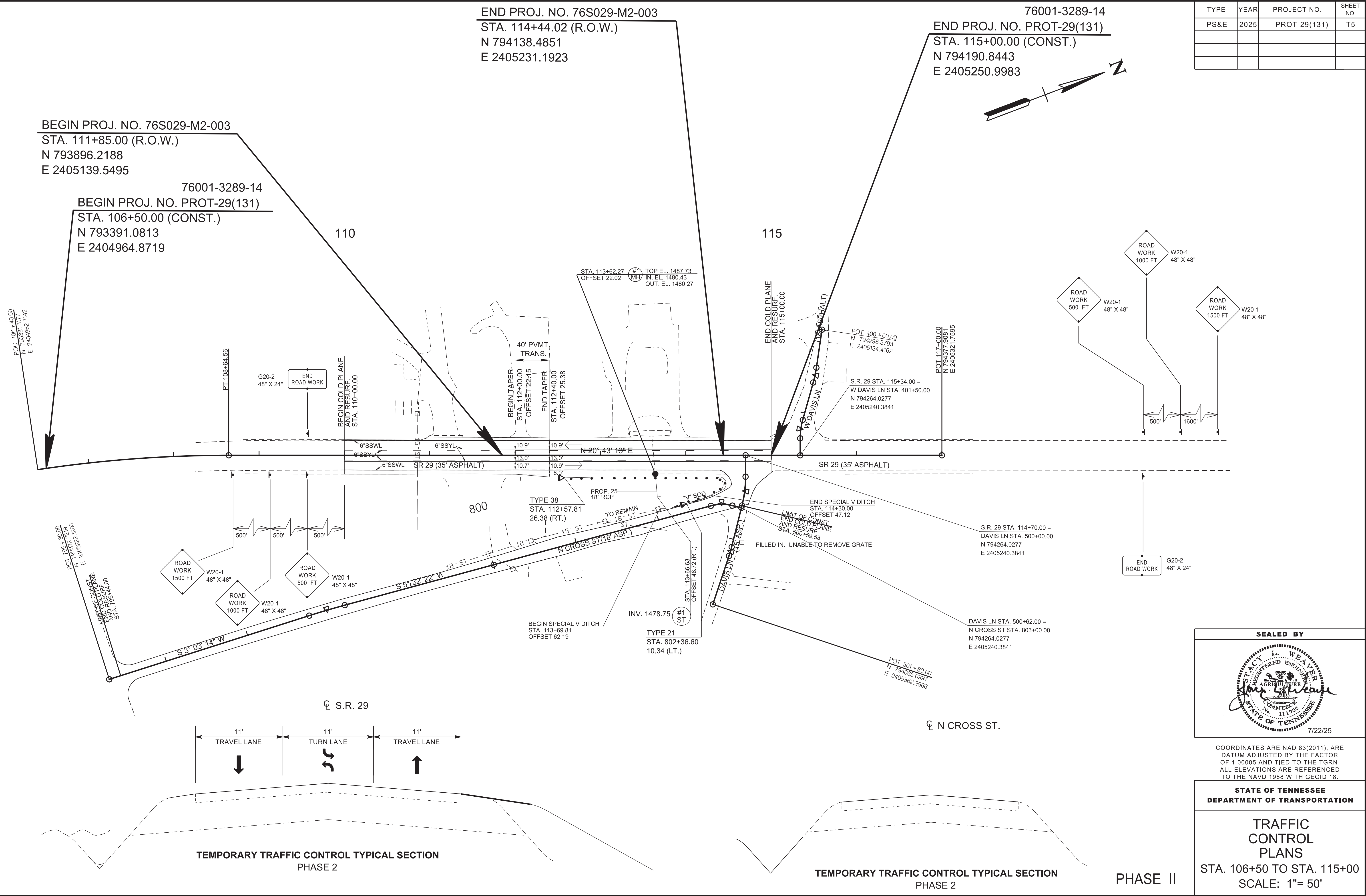
COORDINATES ARE NAD 83(2011), ARE DATUM ADJUSTED BY THE FACTOR OF 1.00005 AND TIED TO THE TGRN. ALL ELEVATIONS ARE REFERENCED TO THE NAVD 1988 WITH GEOID 18.

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC
CONTROL
PLANS
STA. 106+50 TO STA. 115+00
SCALE: 1"= 50'

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CELL BDRSG

\$\$\$\$SYTIME\$\$\$\$\$\$\$\$
\$\$\$\$DGN\$PE\$\$\$\$\$\$\$\$

GEOTECHNICAL INDEX

SHEET NAME	SHEET NO.
SIGNATURE SHEET	GEOTECH-SIGN1
GEOTECHNICAL INDEX.....	G-1
GEOTECHNICAL MATERAIL DEFINITIONS.....	G-2
GEOTECHNICAL BORING LAYOUT.....	G-3
GEOTECHNICAL BORING PROFILE.....	G-4

TYPE	YEAR	PROJECT NO.	SHEET NO.
PS&E	2025	PROT-29(131)	G-1

STATE OF TENNESSEE
DEPARTMENT OF
TRANSPORTATION

GEOTECHNICAL

INDEX

TYPE	YEAR	PROJECT NO.	SHEET NO.
PS&E	2025	PROT-29(131)	G-2

DEFINITION OF EARTHWORK TERMS

THE TERMS AND DEFINITIONS BELOW SHALL CHARACTERIZE THE MATERIAL TYPE THAT WILL BE ENCOUNTERED DURING EXCAVATION AND GRADING. SEE TYPE MATERIAL REFERENCE IN TYPICAL SECTIONS LEGEND.

A. SOIL MATERIAL

SOIL MATERIAL IS MATERIAL THAT IS PREDOMINANTLY MADE UP OF NATURALLY OCCURRING MINERAL PARTICLES WHICH ARE FAIRLY READILY SEPARATED INTO RELATIVELY SMALL PIECES, AND IN WHICH THE MASS MAY CONTAIN AIR, WATER OR ORGANIC MATERIALS. THIS MATERIAL MAY CONTAIN ROCK PIECES IN THE FORM OF DISCONNECTED SLABS, LENSES, OR BOULDERS OF LESS THAN APPROXIMATELY 0.5 CUBIC YARDS. THE MAIN SOIL GROUPS CONSIST OF CLAY, SILT, SAND, GRAVEL, COBBLES, BOULDERS (LESS THAN 0.5 CUBIC YARD VOLUME) OR A COMBINATION OF ANY OF THE CONSTITUENTS. FOR CONSTRUCTION PURPOSES, THIS MATERIAL WOULD TYPICALLY BE CONSIDERED TO BE EXCAVATABLE BY CONVENTIONAL EXCAVATION MACHINERY SUCH AS PANS, TRACK HOES, OR FRONT END EXCAVATORS/LOADERS.

B. SOLID ROCK MATERIAL

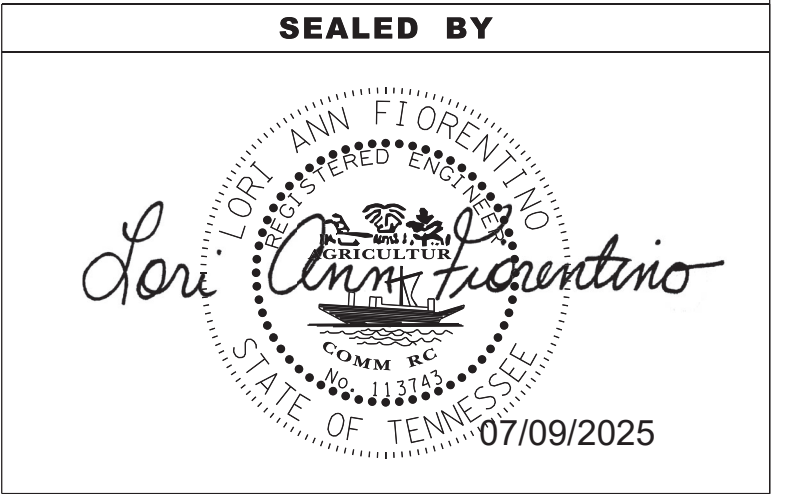
SOLID ROCK MATERIAL IS THAT NATURALLY OCCURRING MATERIAL COMPOSED OF MINERAL PARTICLES SO FIRMLY BONDED TOGETHER THAT RELATIVELY GREAT EFFORT IS REQUIRED TO SEPARATE THE PARTICLES (I.E. BLASTING OR HEAVY CRUSHING FORCES). FOR CONSTRUCTION PURPOSES, THIS MATERIAL WOULD TYPICALLY HAVE TO BE BLASTED TO SEPARATE INTO PIECES SMALL ENOUGH TO LOAD AND TRANSPORT ON EARTH MOVING TRUCKS AND WHICH WHEN SUBJECTED TO PROPER PRE-SPLIT AND PRODUCTION BLASTING WOULD RESULT IN A UNIFORM STABLE ROCK CUT FACE. NOTE THAT THIS MATERIAL WOULD NOT BY DEFINITION NECESSARILY BE A PROVEN SOURCE OF ANY ROCK TYPE AGGREGATE SUCH AS SOLID ROCK, GRADED SOLID ROCK, RIP RAP, OR OTHER ROCK AGGREGATE CONSTRUCTION PRODUCTS.

C. SOFT ROCK OR DEGRADABLE ROCK

THIS MATERIAL IS THAT NATURALLY OCCURRING MATERIAL COMPOSED OF MINERAL PARTICLES THAT ARE SO FIRMLY BONDED SUCH THAT THEY ARE NOT FAIRLY READILY SEPARATED INTO SMALL PIECES YET HAS SUCH RELATIVELY LOW BONDING STRENGTH THAT WOULD ALLOW FOR SEPARATING INTO SMALL PIECES THROUGH MODERATE TO HEAVY CRUSHING FORCES. FOR CONSTRUCTION PURPOSES THIS MATERIAL WOULD HAVE TO BE SUBJECTED TO RIPPING TYPE EQUIPMENT, HOE RAMS, OR RUGGED USE OF A LARGE BULLDOZER IN ORDER TO SEPARATE THE MATERIAL SUCH THAT IT CAN BE READILY LOADED INTO EARTH MOVING TRUCKS. THESE MATERIALS WOULD TYPICALLY BE SHALES, CLAYSTONES, SILTSTONES, WEATHERED SANDSTONES, WEATHERED SCHIST AND WEATHERED GNEISS.

D. TRANSITIONAL MATERIALS

THIS MATERIAL IS THAT MATERIAL COMPRISED OF A COMBINATION OF SOIL AND ROCK (MATERIALS A, B, AND C) OCCURRING IN EITHER NON-UNIFORM INTERBEDDED LAYERS OF THE ABOVE MATERIALS (I.E. SHALE MATERIAL WITH RELATIVELY THIN LAYERS OF SOLID ROCK SUCH AS HARD LIMESTONE) OR ERRATIC LOCALIZED CHANGES OF MATERIAL TYPES BOTH Laterally AND WITH DEPTH (SUCH AS A GEOLOGIC FORMATION RESULTING IN PINNACLED ROCK COLUMNS, FLOATING BOULDERS OR LENSES INTERCALATED WITH CLAY SOIL, A COMMON OCCURRENCE IN CERTAIN REGIONS OF TENNESSEE). FOR CONSTRUCTION PURPOSES, THIS MATERIAL MAY HAVE TO BE EXCAVATED USING A COMBINATION OF EXCAVATION METHODS SUCH AS BLASTING OF ROCK PINNACLES, LAYERS OR BOULDERS ALONG WITH A RIPPING OF WEATHERED ROCK AND EXCAVATING OF SOIL WITH TRACK HOES OR LOADERS ALL WITHIN A LOCALIZED AREA. THIS MATERIAL WOULD NOT BE SUITABLE FOR THE USE OF EXCAVATING PAN TYPE EQUIPMENT.

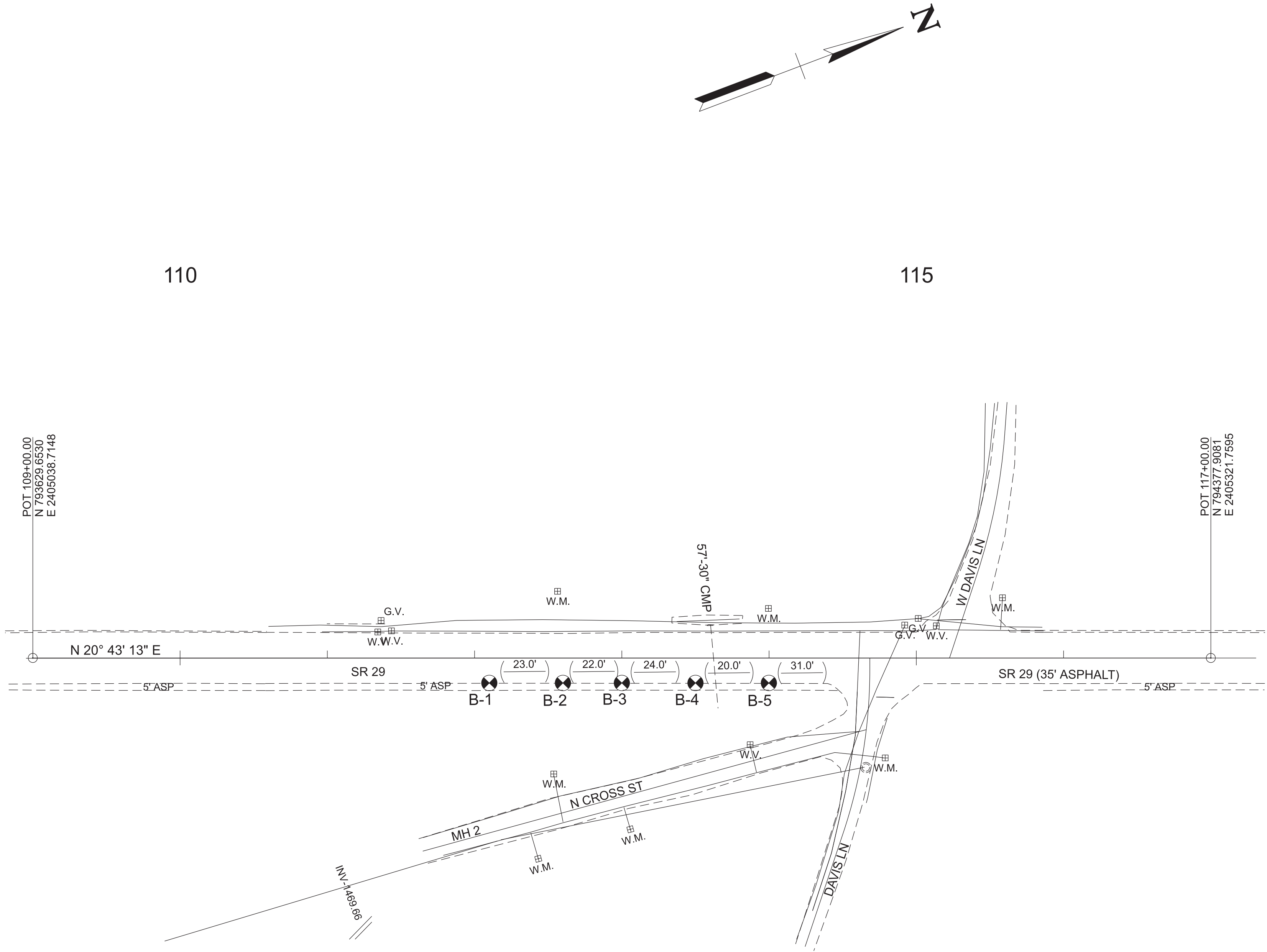


STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

GEOTECHNICAL MATERIAL DEFINITIONS

\$\$\$SYTIME\$\$\$\$\$
 \$\$\$DGN SPEC\$\$\$\$\$

TYPE	YEAR	PROJECT NO.	SHEET NO.
PS&E	2025	PROT-29(131)	G-3



LEGEND

- B-1 (24.5') (70.3') BORING LOCATION. DEPTH TO REFUSAL (ABOVE LINE), BOTTOM OF HOLE (BELOW LINE)
- B-1 (24.5') BORING LOCATION WITH DEPTH TO REFUSAL
- B-1 (24.5') BORING LOCATION TERMINATION DEPTH (NO REFUSAL)

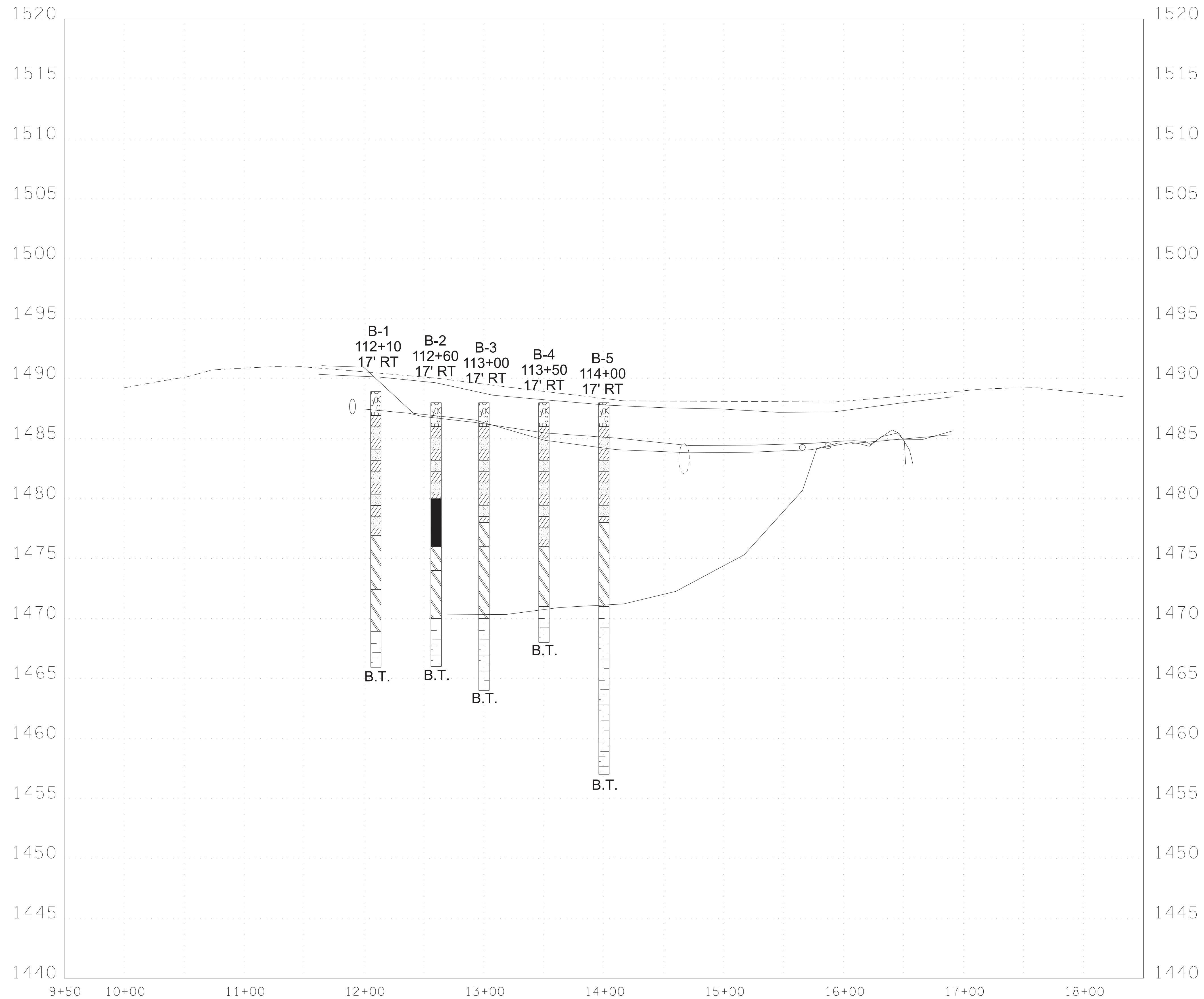
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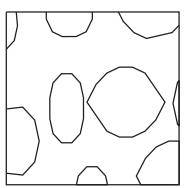
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

GEOTECHNICAL
BORING
LAYOUT

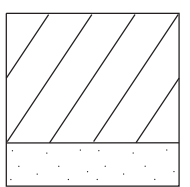
TYPE	YEAR	PROJECT NO.	SHEET NO.
PS&E	2025	PROT-29(131)	G-4



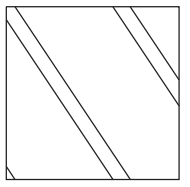
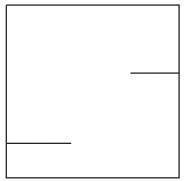
LEGEND



ASPHALT



FILL (TYPE
A MATERIAL)

CLAY (TYPE
A MATERIAL)

WEATHERED SHALE
(TYPE D MATERIAL)

TYPE MATERIAL-SEE DEFINITION
OF EARTHWORK TERMS ON
NOTES AND GEOTECHNICAL EST.
QTYS. SHEET.

B.T.= BORING TERMINATED

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07/09/2025

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TO THE NAVD 1988 WITH GEOID 18.

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

GEOTECHNICAL BORING PROFILE

UTILITIES INDEX	
SHEET NAME	SHEET NUMBER
UTILITIES INDEX, UTILITY OWNERS	U1 SERIES
HIGHLAND TELEPHONE COOP. (TELEPHONE)	

STATE OF TENNESSEE

DEPARTMENT OF TRANSPORTATION

BUREAU OF ENGINEERING

SCOTT COUNTY

SR-29 (US-27):M20SAHQ_R1SR_SR029_18.75 LM_SLOPE CORRECTION

UTILITIES

STATE HIGHWAY NO. 29 U.S. ROUTE NO. 27

UTILITIES NOT IN ROADWAY CONTRACT			
ELECTRIC:	PLATEAU ELECTRIC COOPERATIVE P.O. BOX 4669 16200 SCOTT HIGHWAY ONEIDA, TN 37841 JOEL MCCARTT (423) 569-8591 (NO CONFLICT)	GAS:	CITIZENS GAS UTILITY DISTRICT OF SCOTT AND MORGAN COUNTIES P.O. BOX 320 12519 SCOTT HIGHWAY GREG BELL (423) 569-4457 (NO CONFLICT)
COMM:	HIGHLAND TELEPHONE COOPERATIVE P.O. BOX 119 7840 MORGAN COUNTY HWY SUNBRIGHT, TN 37872 STEVE GARRETT (423) 628-2121 (NO COST - MOVE PRIOR)	COMM:	COMCAST 5720 ASHEVILLE HWY. KNOXVILLE, TN 37924 JASON KELLY 865-437-0295 (NO CONFLICT)
WATER:	ONEIDA WATER & SEWER 19922 ALBERTA STREET PO BOX 4237 ONEIDA, TN 37841 STEPHEN OWENS (423) 539-2322 (NO CONFLICT)	SEWER:	ONEIDA WATER & SEWER 19922 ALBERTA STREET PO BOX 4237 ONEIDA, TN 37841 STEPHEN OWENS (423) 539-2322 NO CONFLICT

UTILITIES IN ROADWAY CONTRACT	
N/A	N/A

TENN.	YEAR	SHEET NO.
	2025	U1-1
FED. AID PROJ. NO.	PROT-29(131)	
STATE PROJ. NO.	76001-3289-14	

STATE OF TENNESSEE DEPARTMENT OF TRANSPORTATION
UTILITY INDEX, UTILITY OWNERS

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2405500

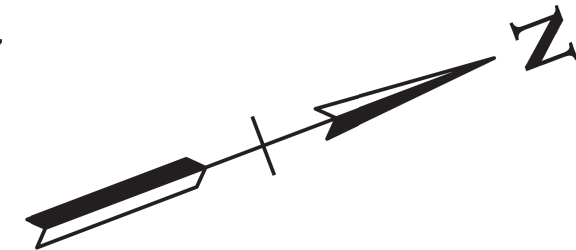
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N 793896.2188
E 2405139.5495

BEGIN PROJ. NO. 76001-4288-04
STA. 110+00.00 (CONST.)
N 793723.1848
E 2405074.0954

END PROJ. NO. 76S029-M2-003
STA. 114+44.02 (R.O.W.)
N 794138.4851
E 2405231.1923

END PROJ. NO. 76001-4288-04
STA. 114+50.00 (CONST.)
N 794144.0801
E 2405233.3087

HTC to remove (2) strands with copper and raise (2) strands with fiber to allow a higher ground clearance. Removals will have to extend beyond the project area to allow proper support of the poles



APPROVED
By Macario Reyes at 12:11 pm, Feb 13, 2023

Going 1,240' beyond the end of Project

Going 3,345' beyond the end of Project

PROPERTY OWNER LIST

#	OWNER NAME
1	PATRICIA R. MINOR
2	EUGENE OLMSTEAD AND WIFE, MAE OLMSTEAD
3	MARY ANN DELK FROMAN, JIMMY BRUCE DELK, PHYLLIS D. DELK JOHNSON, ERIC WAYNE DELK, AND JOSHUA CORY SIBLEY (WITH LIFE ESTATE JESSE DELK, JESSIE NELL DELK SIBLEY AND HUSBAND, JOSEPH C.SIBLEY)
4	ANNA LOU STEPHENS
5	TIMOTHY WAYNE WEST
6	BERNARD TERRY
7	TRACOB, LLC
8	FELICIA GATES, ET AL
9	DARRELL DUNCAN, AND WIFE, CONNIE DUNCAN
10	LESSIE CHAMBERS AND DAUGHTER, MARTHA LOU CHAMBERS
11	MELISSA ANN CHAMBERS
12	JAMES SCOT BOWLING
13	J.J. DOWER AND WIFE, TRACIA DOWER
14	ROBERT WRIGHT AND AMY WRIGHT

CONTROL POINTS					
POINT	NORTH	EAST	ELEV.	STATION	OFFSET
CP48	793744.4660	2405035.8030	1497.01	110+06.36	43.34' (LT)
CP49	794248.2040	2405311.9130	1488.09	115+75.20	36.68' (RT)

CONTROL POINTS IN REFERENCE TO SR29

W DAVIS LN
PI 400+53.89
N 794,272.3710
E 2,405,181.5013
Δ 7° 17' 28" (RT)
D 40° 55' 32"
R 140.00
L 17.82
T 8.92
PC 400+44.97
PT 400+62.79
BK BEARING S60°33'56"E
AH BEARING S53°36'28"E

W DAVIS LN
PI 401+19.26
N 794,233.5717
E 2,405,234.1424
Δ 15° 40' 19" (LT)
D 71° 37' 11"
R 80.00
L 21.88
T 11.01
PC 401+08.25
PT 401+30.13
BK BEARING S53°36'28"E
AH BEARING S69°16'47"E

DAVIS LN
PI 500+42.17
N 794,147.8200
E 2,405,279.8168
Δ 13° 58' 11" (RT)
D 40° 55' 32"
R 140.00
L 34.13
T 17.15
PC 500+25.02
PT 500+59.15
BK BEARING S69°13'06"E
AH BEARING S55°14'55"E

DAVIS LN
PI 501+14.68
N 794,106.3965
E 2,405,339.5256
Δ 4° 27' 17" (RT)
D 40° 55' 32"
R 140.00
L 10.89
T 5.45
PC 501+09.23
PT 501+20.12
BK BEARING S55°14'55"E
AH BEARING S50°47'38"E

N CROSS ST
PI 802+76.67
N 794,116.7895
E 2,405,282.6258
Δ 29° 12' 43" (RT)
D 114° 35' 30"
R 50.00
L 25.49
T 13.03
PC 802+63.64
PT 802+89.13
BK BEARING N34°45'05"E
AH BEARING N5°32' 22"E

R.O.W.
PLANS

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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PRESENT
LAYOUT

STA.110+90.00 TO STA.114+44.02
SCALE: 1"=50'

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

UTILITIES
REFERENCE

UTILITY REFERENCE - HIGHLAND TELEPHONE COOPERATIVE - TELEPHONE
(NO COST - MOVE PRIOR; NOT TO SCALE)